



Proposed Part 8 Residential Development
Bannow Road, Cabra West, Dublin 7

Architect's Design Report

24005-MCORM-AR-RP-9000
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ARCHITECTURE
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1.0 Introduction

This Design Statement has been prepared for Clúid Housing Association on behalf of Dublin City Council (DCC), to accompany a Part 8 proposal for the development of 144 no. residential units on a site of circa 1.16 hectares, located at Bannow Road, Cabra West, Dublin 7. It describes the architectural design of the proposed development, which has been developed in collaboration with the multi-disciplinary project team.

The proposed development will consist of the following:

- Demolition of the existing single storey industrial building, which has a gross floor area (GFA) of 1,455 sq.m;
- Construction of a residential development with a total of 144 no. apartments (71 no. 1 beds, 63 no. 2 beds, and 10 no. 3 beds) in two residential buildings (Block A and B);
- Block A is part 3 to part 6 storeys in height and includes 40 no. apartments (18 no. 1 beds, 21 no. 2 beds and 1 no. 3 bed). Balconies and terraces are provided on all elevations;
- Block B is part 3 to part 6 storeys in height and includes 104 no. units (53 no. 1 beds, 42 no. 2 beds and 9 no. 3 beds). Balconies and terraces are provided on all elevations;
- Provision of residential courtyards at surface level to serve the apartments;
- 25 no. car parking spaces to serve the apartments, 4 no. car share spaces and 3 no. drop-off / loading bay spaces;
- 227 no. long stay and 72 no. short stay bicycle parking spaces to serve the apartments;
- Provision of 420 sq.m of community/cultural floorspace in Block A (380 sq.m internal space at ground floor level and 40 sq.m of associated external area);
- Provision of 78 sq.m of community floorspace at ground floor level of Block B;
- Provision of 2 no. long stay and 6 no. short stay bicycle spaces to serve the community/cultural uses;
- Provision of public open space including central area of public open space and public realm improvements;
- Boundary treatments, public lighting, site drainage works, green roofs, PV panels, internal road surfacing and footpaths, ESB meter rooms, ESB substation, utility and services connections, bin and cycle storage, plant rooms, landscaping and levelling of ground reserved for proposed Luas extension / operations; and
- All associated development.

A coordinated Pre-Part 8 consultation process was undertaken with all relevant departments of Dublin City Council, including Planning, Housing, Area Office, Parks, Drainage, Transport, Conservation, Archaeology, and the City Architect's Division. The comments received have informed the final Part 8 proposal, and has been carefully shaped by its urban context, planning policy and site constraints.

Project Team:

<i>Approved Housing Body</i>	- Clúid Housing
<i>Contractor</i>	- GEM Construction
<i>Planning Consultants</i>	- John Spain Associates (JSA)
<i>Architects</i>	- MCORM Architecture and Urban Design (MCORM)
<i>Civil Engineers</i>	- OCSC Consulting Engineers (OCSC)
<i>M&E</i>	- OCSC Consulting Engineers (OCSC)
<i>Landscape</i>	- CSR Landplanning & Design (CSR)
<i>Ecology</i>	- DNV
<i>Daylight/ Sunlight</i>	- 3D Design Bureau (3DDB)

2.0 Site Location and Description

2.1 Location

The subject site is located along the Railway Line / Royal Canal in Cabra close to the Broombridge Luas/ Train stop, in the Northwest of the city. The site is currently vacant and was previously used as an industrial site.

The location is highly accessible for public transport. The LUAS Green line and Dublin-Maynooth/Longford/M3 Parkway train are less than a 10 minute walk. High quality bus services are available close by on Carnlough Road and Faussagh Avenue (3-6 minute walk). Cabra Road and Navan Road are also less than a 20 minute walk (two main arteries in and out of the city centre). The city centre (O'Connell street area) is also within the 15 minute cycle distance.

The site is strategically located within the canal boundaries with several notable amenities, landmarks and facilities close by including:

Amenities:

- Phoenix Park
- Dublin Zoo
- Royal Canal Greenway
- Tolka Valley Park
- Cabra Community Centre and Park
- Faussagh Avenue Local Shops & Retail
- Navan Road Retail Centre
- Local sporting clubs/pitches -St Finbarr's GAA, Beggsboro AFC, TUD Dublin Broombridge

Landmarks and Facilities:

- An Post Depot Bannow Road
- Broombridge Educate Together school
- Cabra Community College
- Cabra National Schools
- Batchelors factory complex
- Ballyboggan Industrial Estate
- Glasnevin Cemetery
- Cabra Church

The diagrams opposite indicate relevant walking and cycling travel distances. In particular there is a dominant directional walking bias from the site to the south towards Cabra / Navan Road etc due to the constraints posed by the railway line and canal that bounds the site to the north.



Image 1 - Site Location Aerial View

- LUAS LINE
- TRAIN LINE
- DCC ACTIVE TRAVEL ROUTES
- ADDITIONAL BUS ROUTES

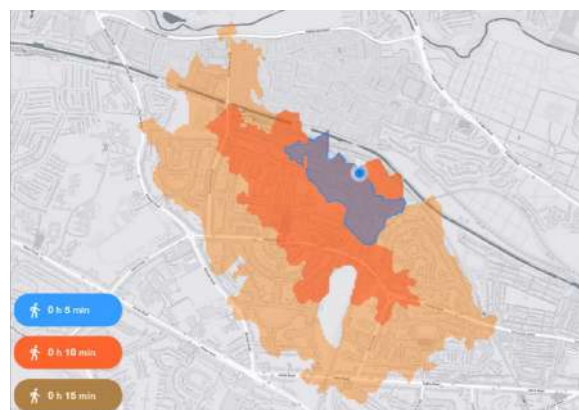


Image 2 - Walking distance Diagram
(The railway lines restrict travel times to the north)

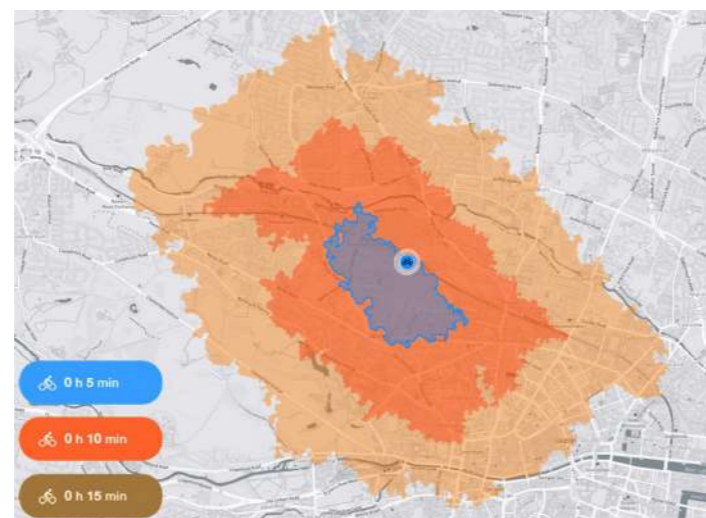


Image 3 - Cycling distance Diagram



Image 4 - View of Bannow Road - Showing typical Terrace housing in Cabra

2.2 Site Context - Cabra West

Cabra as an area began to be developed from the 1920's when Dublin Cooperation began to develop swathes of land for large housing schemes in a direct response to the slums, dereliction and housing supply issues of the inner city.

The Cabra Housing Scheme was developed in three phases as part of a programme of planned working-class suburbanization. The masterplan was influenced by the early British Town-planning movement with large areas of two storey houses set out in formal geometric patterns with small pocket parks and standalone community buildings (Churches and Schools) placed at key locations to act as landmarks within the urban grain.

The 'two up-two down' mass concrete housing typologies built in this area are synonymous with Dublin and are seen in many parts of the city redeveloped in that era.

The site for this application is located at the edge of Cabra West in the zone of land between Bannow Road and the Railway / Royal Canal. The adjoining Batchelors Food factory complex originating in the late 1930s is a notable landmark in the area.

This part of Cabra has been reactivated in recent years with the LUAS. Recent redevelopment includes a new housing development close to the LUAS stop and Broombridge Educate Together school.



Image 5 - Aerial View of the site within the surrounding context



Image 6 - Site Aerial Photo

2.3 Site History

The historic OS maps below describe the development of the area from the arrival of the Royal Canal in the 1790's and then the train in the 1880's, through to the beginnings of the Cabra Housing scheme in the 1920's and up to the present day.

The zone of land along the railway lines was originally set aside for non residential use. The subject site has an industrial nature, gated and secured and all sides by tall boundaries. It appears to have been previously occupied by a number of factory type structures. Most of the buildings have been cleared expect for a single storey building in the southern corner. This was part of the Premier Chicks Factory, an egg hatchery complex that occupied the site from 1949. The building was designed by architectural practice Jones & Kelly.

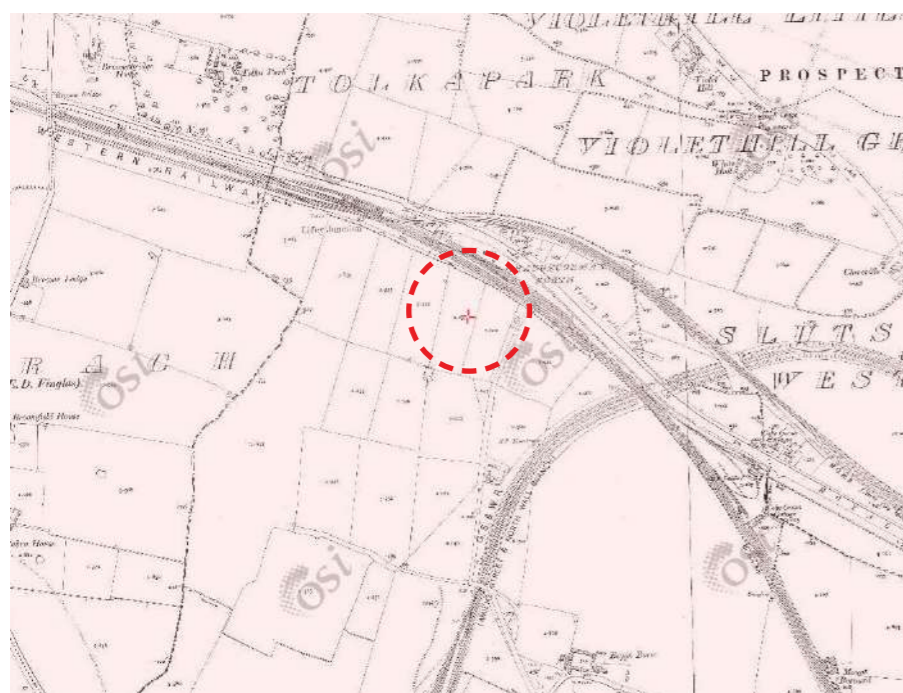


Image 7 Left- Ordnance Survey 6 inch First Edition Map - 1880s

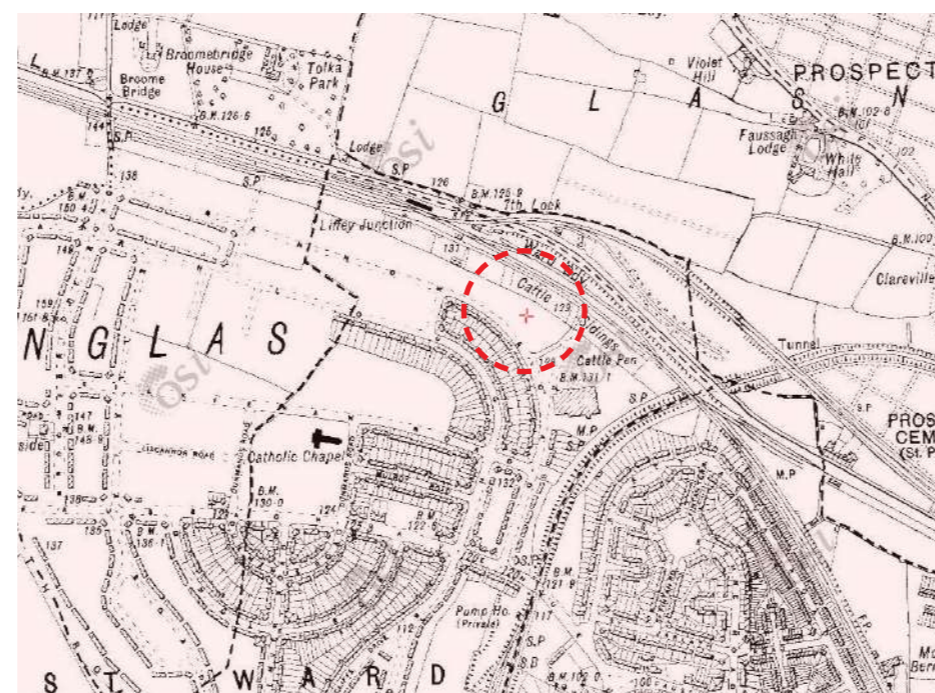


Image 8 Middle - Ordnance Survey 25 inch Map - 1920s

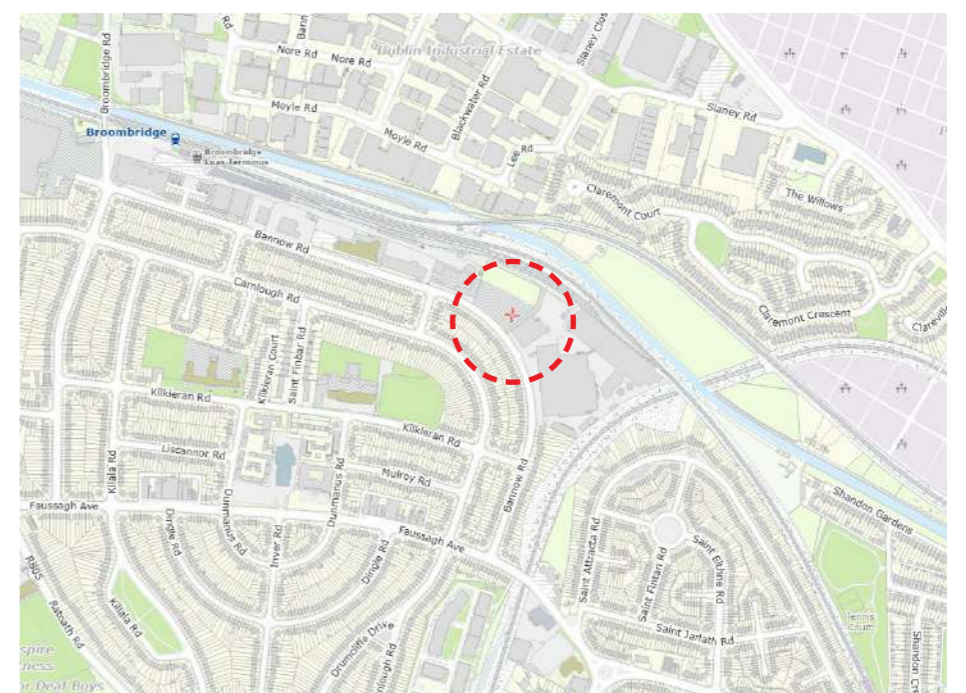


Image 9 Right - Ordnance Survey 6 inch Last Edition Map - 1950s

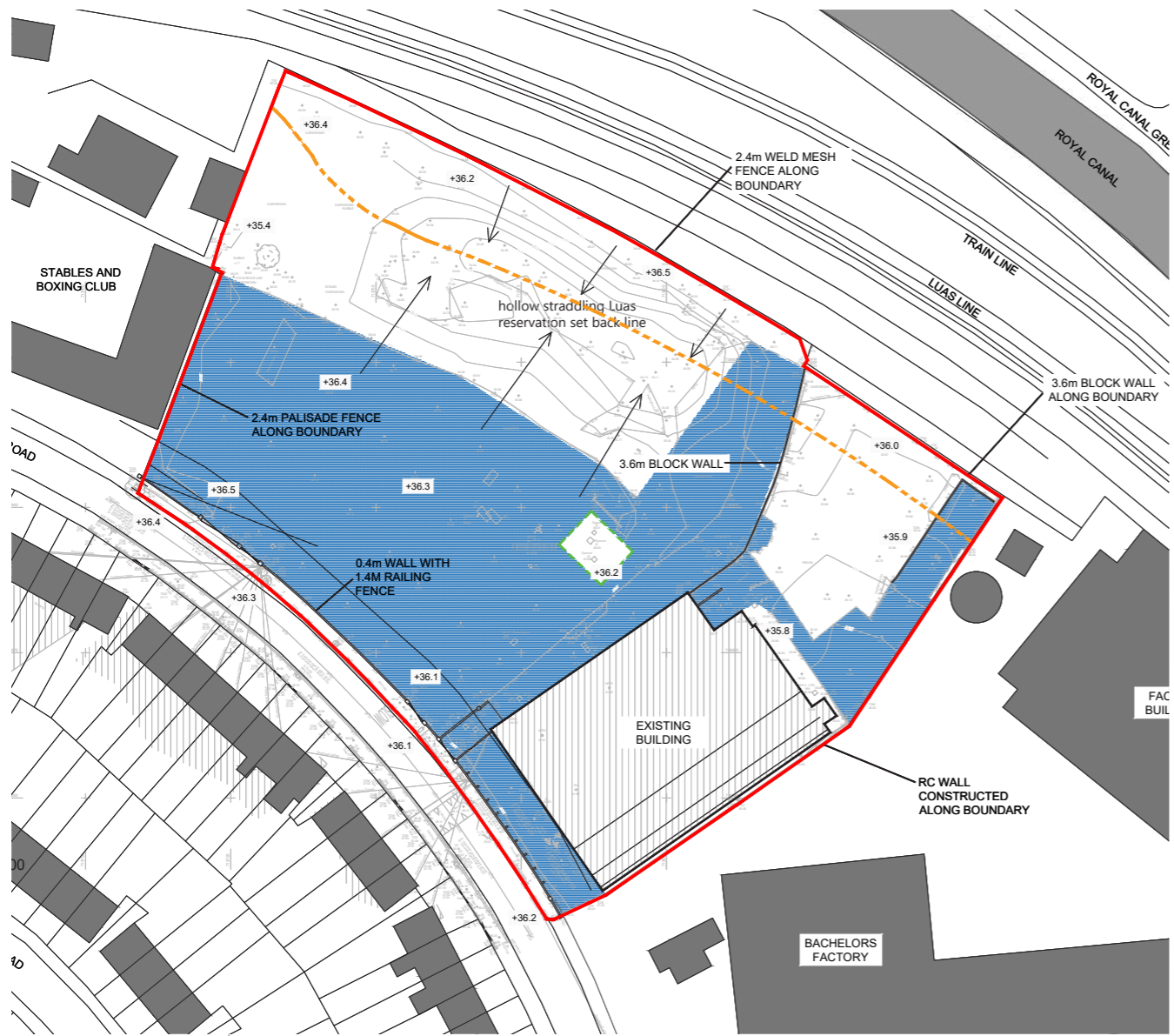


Image 10 - Existing Site Plan

- - - Existing Tank
- Existing Concrete Slab

2.4 Site Description

The site is currently vacant. As noted, the previous industrial buildings that covered the site have been mostly demolished except for one building located in the southern corner. The majority of the area is covered with hard-standing with incidental areas of scrub due to lying idle. The site investigation works identified a layer of made ground below the surface and evidence of a concrete tank substructure in the south/centre of the site.

The site fronts onto Bannow Road along the south west with two storey housing to the opposite side of the street. The Batchelors factory complex bounds the site to the south east and a Stables and Boxing Club backs onto the site to the north west. The Luas and Irish Rail lines bound the site to the north east with the Royal Canal and Canal Greenway beyond.

The site slopes towards the centre and away from both Bannow Road and the railway lines creating a low point of approx. 2.4m towards the north/centre. Foul and surface water drainage are located on Bannow Road which is higher and the proposed development will need to tie into existing drainage levels on this street.

There is mixed security fencing along the boundary to Bannow Road consisting of railings, brick piers and barbed wire. There is gated vehicular access in two locations. The boundary to the west consists of a 2.4m high Palisade fence and the boundary to the east is a 2 -2.4m high reinforced concrete wall. The boundary to the railway line consists of a part 2.4m high weld-mesh fence and part 3.6m high block wall.

Please see site photos overleaf.



Image 11 - Existing Weldmesh fence boundary to the railway lines (the site slopes down into a hollow)



Image 12 - Existing block wall boundary to the railway line (left) and newer concrete wall boundary to Batchelors site with silo structure behind (right)



Image 13 - Palisade fence boundary to the Stables and Boxing Club site



Image 14 - View of site frontage from the north on Bannow Road



Image 15 - View of site frontage from the south on Bannow Road - factory building on the right



Image 16 - LUAS Expansion plan (new stabling lanes in orange/grey)

2.5 LUAS expansion

There are plans for expansion of the Luas facilities at Broombridge. It is part of the wider plans to expand the Green Line to Finglas. The proposal consists the construction of 4 no. additional stabling lanes for parked/ idle trams.

The area of expansion encroaches on the site area along the northern boundary. A non developable zone is reserved to facilitate the plans.

As mentioned there is a significant hollow in north/centre of the site and we have included the reserved area in the overall red line so that levels can be brought up to grade to facilitate the construction of a proper boundary treatment between the proposed development and the future expanded Luas lands and to avoid the requirement for extensive retaining walls / structures.

The zone reserved for the Luas expansion is excluded from the site net area calculations.



Image 17 - LUAS Expansion 3d image (dashed red line marks the edge of proposed development)



Image 18- Aerial 3d view of the site and surrounding buildings

2.6 Bannow Road

Bannow Road is a typical suburban street. The carriageway is approximately 6.5 metres wide and finished in concrete with footpaths either side. To the south, there are two storey houses in terraces of 4-8 units - all with front gardens / driveways to the street. The north side is occupied by larger industrial and community type buildings. Parking often takes place informally on street / on footpaths. The character of the street is quite utilitarian with a general lack of on street greening or soft landscaping.

2.7 Surrounding Buildings

- The 2 Storey Houses on Bannow Road are typical two up two down terraced houses with wet dash finish and evidence of the usual elevational modifications and upgrades. The houses directly opposite the subject site have stepped access and all sit above the level of street (approx 600mm).
- The Stables and Boxing Club to the west of the site comprises of a complex of inward looking single storey shed buildings and a central courtyard type space. The site is used for horses stables and one building appears to be a boxing club / gym. The site has blank facades onto Bannow Road and the subject site with a single vehicular / pedestrian entrance.
- The Batchelors Factory site consists of a series of one and two storey buildings and large industrial sheds. There is a unique saw toothed roof building bounding onto the railway line. Primary access to the grounds is further south on Bannow Road within a secondary gated / emergency access located closer to the subject site.



Image 19 left - Stables and Boxing club



Image 20 centre - Batchelors factory -main building



Image 21 right - Bannow Road Housing

3.0 Context and Setting

3.1 Planning and Zoning

The site is zoned Z1 'Sustainable Residential Neighbourhoods' (to protect, provide, and improve residential amenities).

Permissible uses for Z1 zoned lands include:

- Residential
- Community Facility

The site is located within 'City Centre and within the Canal Ring (inner suburbs)'. Appendix 3 of the current DCC Development Plan identifies typical density ranges according to location as per the table below. The range for the Canal Belt is 100-250 units per hectare. Appendix 3, Section 4 states that the general principle is to support increased height and higher density schemes, with at least 6 storeys at street level as a default objective in areas within the canal ring of Dublin. The proposed building heights of 3 to 6 storeys has had regard to the neighbouring buildings and the proposed heights are considered to be consistent with the height policy for sites within the Canal Ring (inner suburbs). Notwithstanding that the site is located within City Centre and within the Canal Ring (inner suburbs), as the proposed heights of 3 to 6 storeys exceeds the prevailing height in the area, Appendix 2 of this report outlines compliance with Table 3 of Appendix 3 relating to Performance Criteria in Assessing Proposals for Enhanced Height, Density and Scale

The site is located in the proposed Broombridge-Hamilton Masterplan 2025 zone which takes in the large industrial estate complex to the north of the Royal Canal. This draft masterplan focuses primarily on the Z6 employment lands located to the north of the Royal Canal and rail line, with the stated aim of providing a planning and design framework for their future rezoning to Z14 and transition to predominantly residential-led mixed-use development in line with the compact growth strategy of the Dublin City Development Plan. The masterplan does not seek to rezone or provide a detailed framework for the Z1 residential lands along Bannow Road, including the subject site. These lands will continue to be assessed under the policies and objectives of the Dublin City Development Plan rather than being directed by the masterplan. The subject site falls within Character Area 7 and confirms that no specific masterplan framework (e.g. for land use, building heights, or densities) applies to Bannow Road lands. Instead, proposals for these sites, including the subject site, are to be evaluated directly against the Dublin City Development Plan 2022-2028.

Location	Net Density Range (units per ha)
City Centre and Canal Belt	100-250
SDRA	100-250
SDZ/LAP	As per SDZ Planning Scheme/LAP
Key Urban Village	60-150
Former Z6	100-150

Image 24 - DCC Development Plan Density Range Table Extract

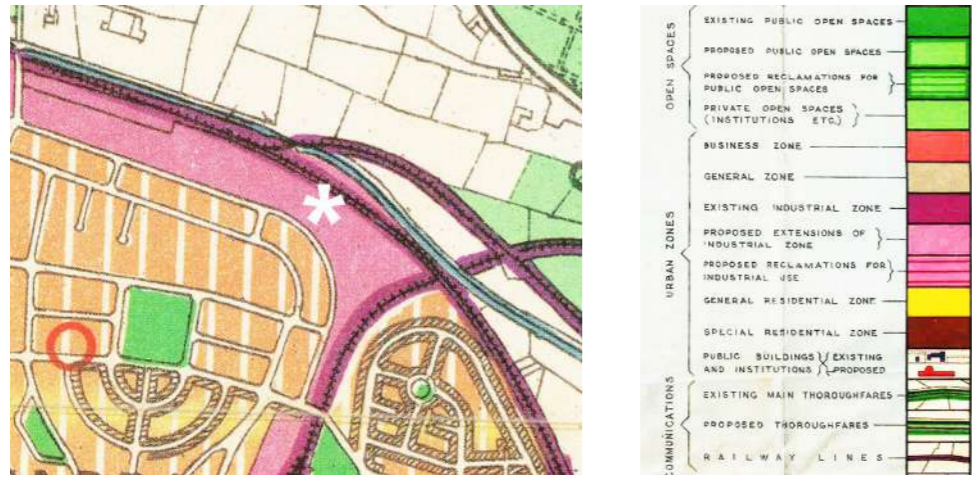


Image 22- Dublin Sketch Development Plan 1941 with zoning legend (Asterix indicates site location)

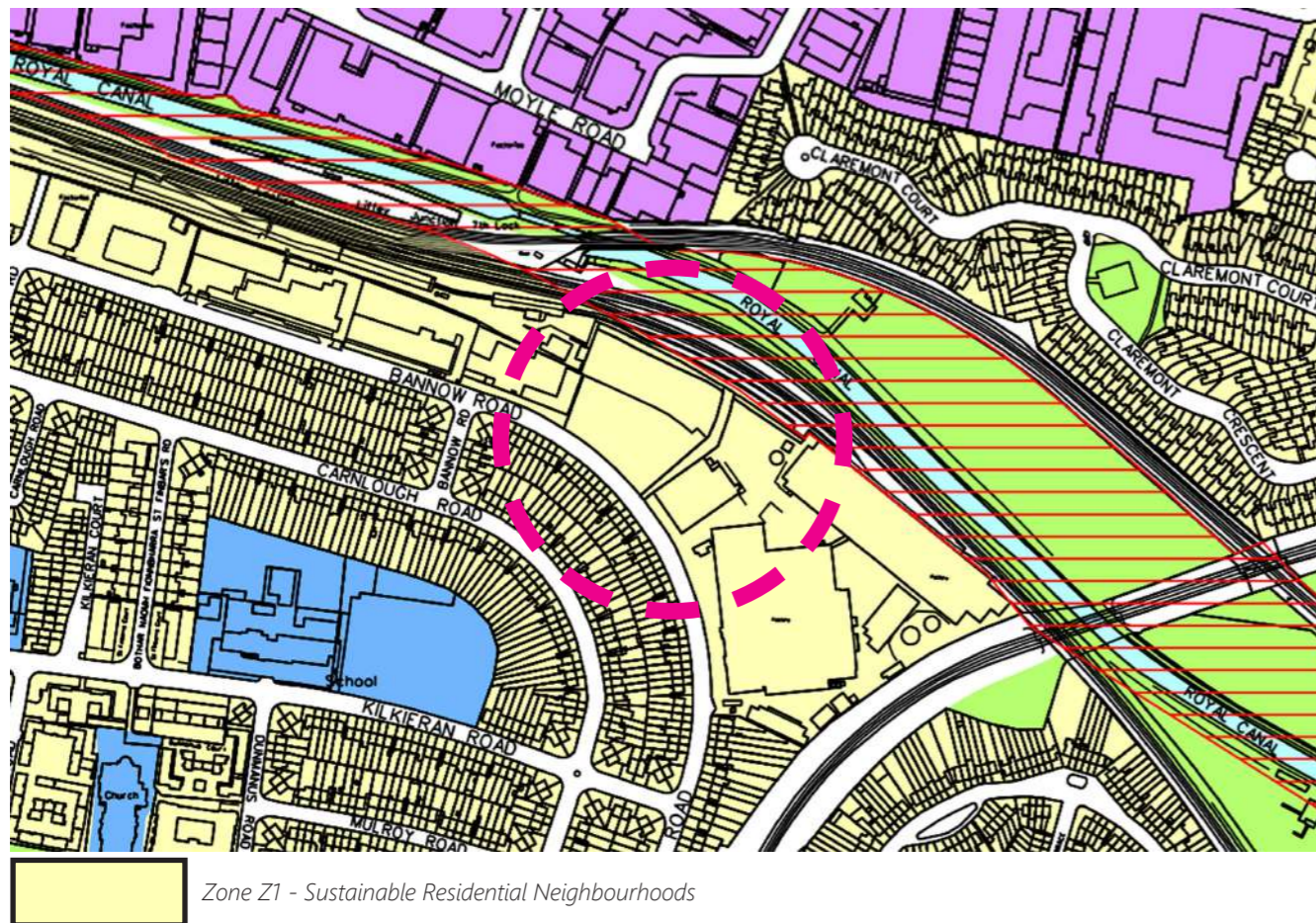


Image 23 - DCC Development Plan 2022-2028 Zoning Map Extract

4.0 Design Evolution / Alternatives Considered

4.1 Brief

A project brief and unit mix was prepared for the scheme by Clúid in consultation with Dublin City Council based on current housing needs and following preliminary site capacity studies. This outline brief is shown opposite.

4.2 Preliminary Appraisal

Our initial appraisal and analysis of the proposed site and context noted the following key points:

- The site is well located close to an existing residential neighbourhood. It is an accessible location with good access to public transport.
- It is an urban infill type site
- There is significant street frontage onto Bannow Road with good sight lines and the potential for new access points as required.
- The site has a particular context. The railway lines and the Royal Canal create large separation distances to the north with significant views to Glasnevin beyond. The sites to the east and west are industrial in nature with a mixture of one and two storey sheds and factory buildings. And then there are the two storey houses opposite on Bannow Road.

When developing an approach to potential development we looked at number of examples of recent developments in a similar context along the railway lines in particular the permission for new apartments on Bannow Road close to Broombridge Station Reg Ref 3308/20, the recent development close by at Hamilton Gardens and Highfield Park and the developments at Pelletstown/Ashtown. We also reviewed the initial feasibility study prepared by Clúid Housing for DCC in 2023.

The appraisal brought up a few key items/ questions to inform the new design:

- The existing context of two storey housing to be respected
- Due to site characteristics - the potential for an apartment scheme to help meet housing demand?
- Potential for improved site permeability
- Allowance for the future LUAS expansion
- Ground floor activity and response to the street
- Appropriate location for a creche and community space
- Approach to car parking? How many spaces are needed if any?
- Site servicing - bin collection / drop off/ deliveries/ turning circles etc. - has emergency vehicle access been considered?

Brief	
One Bed Units	50%
Two Bed Units	40%
Three Bed Units	10%
Total	144 No.
Community Space	

Image 25 - Design Brief

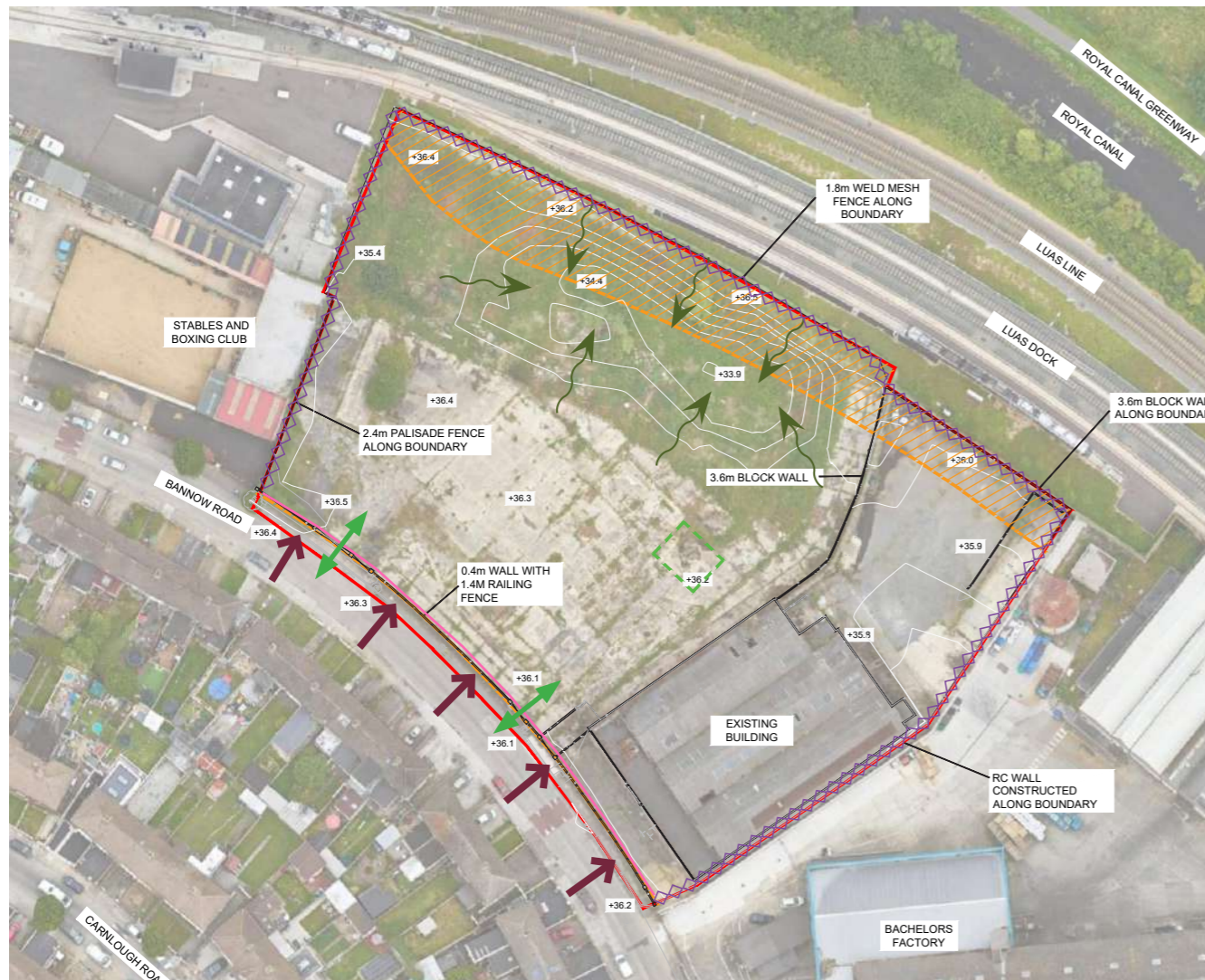
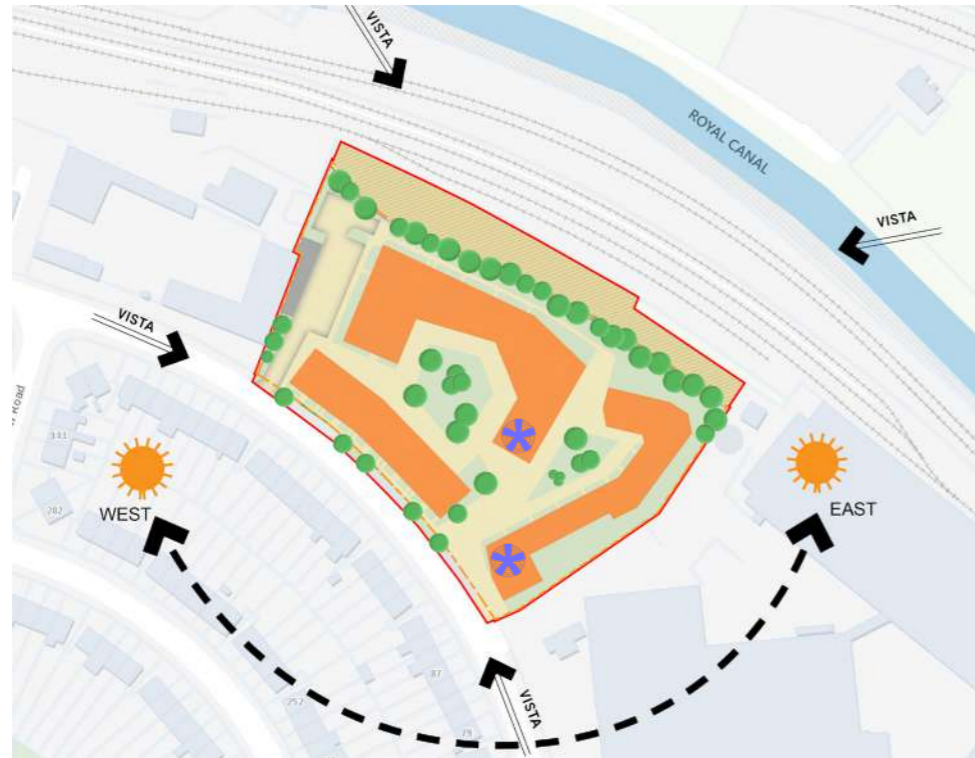


Image 26 - Existing Site Plan

- Application site boundary
- Existing Boundary Walls, Fences
- Existing Site Access
- Area for future LUAS expansion
- Direction of Slope on site
- Main site frontages
- Existing Tank



 Proposed Community Space

Image 27 - Site Strategy Option A



 Proposed Community Space

Image 28 - Site Strategy Option B

4.3 Alternative Designs

In the preliminary design stages we examined a number of sketch proposals for the site and the main alternatives are summarised here.

Clúid Design for DCC

For the original Expression of Interest invitation for the site, Clúid Housing prepared an outline scheme for DCC. The proposal consisted of three blocks of various size enclosing an area of public open space and a communal courtyard space.

A three storey duplex block was positioned on Bannow Road to establish an appropriate scale opposite the existing houses with larger blocks rising to six storeys facing onto the railway tracks. 18 No Parking spaces were provided, located both on-street on Bannow Road and on-site along the western boundary.

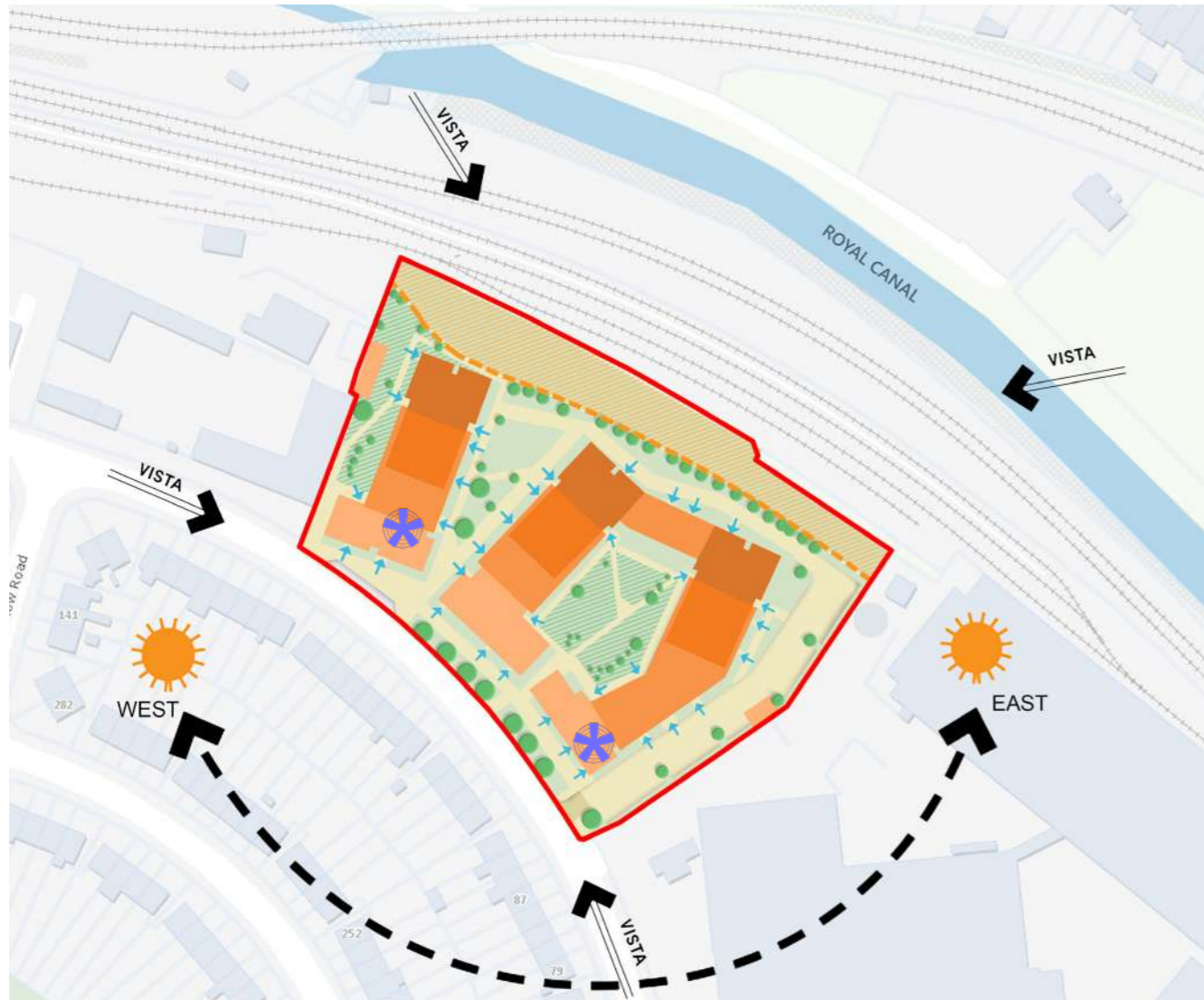
In total 144 units were proposed in a mix of one, two and three bedroom types. There was also a community facility of approx 200 sq.m split in two areas.

Initial Clúid Innovation Partnership Proposal

MCORM first looked at the site in 2023 as part of a submission for the Clúid Innovation Partnership and with the overarching objective to deliver Lean and Green construction.

We looked to expand on the principles of the initial Clúid design with an extra focus on delivering some more standardisation and efficiencies for construction. We looked at the unit types and developed block shapes that became more standard to allow repetition of typologies across the scheme.

We switched the parking within the site from the west boundary replacing it with an area of communal open space with more favourable aspect. We also split one of the blocks in a move to improve permeability within the site. We also considered designating one of the community spaces as a creche. This proposal had a total of 147 units..



 Proposed Community Space

Image 29 - Site Strategy Preferred Option

4.4 Preferred Option

From the middle of 2024 onwards, the scheme was developed in more detail. Through a series of discussions with the wider project team and DCC a preferred site layout emerged.

Two blocks are now proposed. Block A is an L-Shaped block that encloses a communal courtyard along the west boundary with good westerly aspect. Block B is a full courtyard Block with a communal open space in the middle.

An increased set back is now created along Bannow Road to provide additional space for greening along Bannow Road.

A new public open space is created between Block A and B with good enclosure, overlooking and passive surveillance

Own door ground floor units in both blocks provide activation and a sense of ownership to the street.

Access and parking is retained along the east boundary with spaces on Bannow Road now for drop off only.

A new shared street incorporating an emergency access route has been provided around the north of Block B and back to Bannow Road through the new Public Open Space creating a new permeable loop within the scheme.

A coordinated Pre-Part 8 consultation process was undertaken with all relevant departments of Dublin City Council, including Planning, Housing, Area Office, Parks, Drainage, Transport, Conservation, Archaeology, and the City Architect's Division.

Following feedback received, additional community/cultural floorspace has been provided within Block A. Other key updates included the provision of 4 No additional car parking spaces, the provision of brick facades to the courtyard of Block B in lieu of a render treatment.

5.0 Building Layout and Design

5.1 Scheme Design

The final proposal consists of a scheme of 144 apartments together with a community spaces totaling 498 sq.m.

The blocks range in height. The frontage onto Bannow Road is 3 storeys with gable fronts keeping with the scale of the street. The massing rises to 6 storeys along the northern boundary facing onto the train lines creating a strong presence in this location.

A density of 147 units/Ha (net) is proposed.

5.2 Accessibility and Permeability

The development is accessed directly from Bannow Road. Two vehicular access points are proposed - one in the southeast corner and an emergency access between the two blocks.

The new shared surface street around Block B and through the new public open space creates a permeable loop. This facilitates access to both blocks and allows for ground floor activation all round.

Separate pedestrian and cycle access is also provided to each of the communal open spaces.










Site levels are graded to tie into the existing street level and drainage inverts on Bannow Road and to give a proposed floor level for both blocks of +36.6m. This will create a relatively flat site which will facilitate ease of universal access to all parts of the development.



Image 30 - Massing Model



Image 31 - Site permeability diagram

-  Site frontage to
-  Access to COS
-  Access to Community Space
-  Access to Apartments
-  Vehicular Access
-  Pedestrian Access
-  Permeable Loop
-  Own door apartment entrances
-  Community Space

5.3 Scale and Massing

A specific response to site context has informed the approach to height and scale. 3 storey elements are located onto Bannow Road with the heights stepping up to 6 storeys facing the railway lines. The gradation of height is in line with BR209' recommendation for site layout planning in particular a maximum 25° angle of obstruction is easily maintained to the existing dwellings.

A key part of the elevation design has been to try to develop a hierarchy of scale for the facades. The massing of the blocks is strategically proportioned at intervals by using building indents, breaks and recesses to modulate the primary facades into smaller lengths to create a more domestic scale for the buildings. Own door units with front doors and front garden areas give a more individual sense of ownership along the street. The gaps provided between both blocks allow for high levels of daylight and sunlight into the open space areas.

Section 12 of this report expands further on the approach to building height and density. Please also refer to the landscape and visual impact assessment (LVIA) prepared for the application by CSR and 3DDB for verified views that show the building in the existing context.



Image 32- Context Elevation Bannow Road



Image 33 - Context Section



Image 34 - Masterplan

5.4 Masterplan of Adjoining lands

Following early discussions with DCC it was deemed appropriate to consider the proposed scheme in the context of potential future redevelopments of adjoining industrial sites that have a similar Z1 zoning.

We have looked at the sites either side namely the 'stables and boxing club' site to the west and the 'Batchelors site' to the east. We have prepared a masterplan diagram to show how these areas could be reimagined. The lands further west on Bannow Road (An Post and Broombridge Educate Together) offer community facilities for the area and are less likely to be redeveloped. We note that the adjoining sites are in third party ownership and the proposals here are indicative only.

The railway lines create a large obstacle in the urban fabric in this area. The lines act as a hard boundary to the land between Bannow Road and the canal with limited to no opportunity for connection to the Royal Canal Greenway and the Ballyboggan lands beyond.

We suggest that this zone of land south of the railway relates primarily to Bannow Road and the relationship to the existing residential properties in that area is the key part of rethinking the lands.

The stables site is quite modest in size. We suggest it might be suitable for two blocks accessed in the centre. The blocks acting to complete the courtyard enclosure established by the proposal in this application.

The Batchelors site is more extensive and potentially suited to more significant development. We propose a rhythm of blocks similar in scale and proportion to this application. Four blocks are proposed with a new public open space in the centre of the site. A new street scape is proposed within the site with two new access points from Bannow Road.

The proposed frontage set back and street greening in this application is continued across the Batchelors site to create a coherent new streetscape.

While the scope of the plan is quite high level it does suggest the potential for a coherent pattern of development that could unlock this area as a new reimagined extension to the Cabra neighbourhood.

A analysis of the daylight and sunlight potential for blocks on the adjoining sites has been included in the daylight and sunlight report prepared by 3D Design Bureau.



Image 34A- Masterplan Aerial View



Image 35 - View of Block B from Bannow Road (Community space on the corner)

6.0 Open Space

6.1 Landscape Design Strategy

The integration of a high quality landscape proposal is an important part of providing a sustainable development proposal. Landscaping is intrinsically linked with biodiversity, ecology, sustainable drainage systems and the character and quality of a residential area.

Cunnane Stratton Reynolds are the landscape architects for the project and they have developed an extensive landscaping strategy for the scheme including public open space, communal open space, street greening, private open space and green/ blue roofs.

A total of 1450 sq.m of public open space is provided which equates to 14.8% of site area.

A total of 1300 sq.m of communal open space is provided - Block A 450 sq.m, Block B 850 sq.m.

In accordance with the apartment guidelines the minimum requirement for communal open space for a scheme of 71 No. one beds, 58 No. (4p) two beds, 5 No. (3p) two beds and 10 no. three beds is 881sq.m (71 No. x 5 sq.m + 58 No. x 7 sq.m + 5 No. x 6 sq.m + 10 No. x 9 sq.m).

In addition to the above the scheme also contains a shared street with tree planting, permeable surfaces and areas of soft landscaping and a minimum of 50% of the total roof is area is provided with an intensive green roof.

The following sections describe the key aspects of the proposed amenity spaces:

- Public Realm Space - Bannow Road
- Public Open Space
- Communal Courtyards
- Private Open Space



Image 36 - Public Areas Diagram



Image 37 - Communal Open Space Diagram

6.2 Public Realm Space - Bannow Road

The frontage along Bannow Road and the street character has been a key consideration during the design process. We made a conscious decision to set the building line back from the edge of the road to create additional space. This has the benefit of creating a larger set back to the two storey houses opposite as well as providing space for public realm improvements and street greening.

A generous planting zone has been introduced along the road edge, incorporating seating areas and small civic spaces. The soft planting will lift the character of the otherwise hard street scape. We suggest that the treatment can also be carried through in the masterplanning of adjoining sites.

The planting has been deliberately extended to the road edge to discourage footpath parking, a common issue on this type of suburban street, as observed in the surrounding area.



Image 38 - View of Bannow Road

6.3 Public Open Space

A new civic space is created between Blocks A and B. The triangular shape has a strong sense of enclosure and high levels of passive surveillance.

The area is populated with a high quality soft and hard landscaping scheme. It contains an informal green space with mounding and natural play items together with a paved zone with raised planter beds and generous congregation / bench areas for people to sit and enjoy the space. The area has good southerly aspect and the buildings provide good shelter resulting in a pleasant outdoor environment.

A clear defensible zone with front gardens for ground floor units is provided at the building edge to clearly define the public and private spaces.

The triangular space measures 900sqm and together with the public realm plaza improvements along Bannow Road provide a total of 1450sqm of public open space for the scheme.



Image 39- View of Central Public Open Space

6.4 Communal Courtyards

Both Blocks are provided with secure communal open spaces incorporating play areas, green spaces and shrub planting.

Residents have direct access to the spaces via stair cores or the gated access from the street.

Bin stores and bicycle stores area also accessed from the courtyards ensuring additional activity and footfall within the spaces.

Both areas have good access to sunlight, are enclosed and have high levels of passive surveillance.



Image 40 - Block B Communal Courtyard



Image 41 - Typical Balcony



Image 42 - Typical Ground Floor Terrace

6.5 Private Open Space

Private open space is provided via balconies for upper floor units or terraces for ground floor units in accordance with the apartment guidelines.

- 5m² for One Bed two person units (1B2P)
- 6m² for Two Bed three person units (2B3P)
- 7m² for Two Bed four person units (2B4P)
- and
- 9m² for Three Bed five person units (3B5P)

At ground level, own door units also have small front garden areas in addition to the private open space provision. Defensible planting is provided to the public streets. Ground floor terraces are generous and are defined with railings and hedging. Private open spaces directly adjoin living room / kitchen areas.

At upper floors, each apartment has a private balcony. Balconies are generally finished with a vertical railing, to offer visual permeability and overlooking of the streets below, and to the surroundings beyond. Typical private open space treatments are illustrated opposite.

Private open spaces generally have east, west, or south-facing aspect in accordance with the definitions in the apartment guidelines.. There is one location in Block B Core 4 where units feature north-facing balconies. These units have maximised views over the Royal Canal and Glasnevin beyond. See Image on Page 30.

7.0 Community Culture and Arts Space

Two Community, Culture and Art spaces are proposed. Space No. 1 (420 sq.m) in Block A and Space No. 2 (78 sq.m) in Block B. Indicative layouts have been provided for both spaces. It is envisaged that, in the event of Part 8 approval, the applicant will engage with local groups and organisation and invite proposals for the use of the two spaces. Its hoped that the variety in the size /scale of the two spaces will appeal to a wider range of uses.

7.1 Community Space No. 1

This space is located in Block A and measures 420sq.m consisting of 380 sq.m of internal space and 40 sq.m of associated external amenity space. The facility is accessed from the new public open space via a reception area. The space is a multi-room facility and has been organised to set out 4 no. large multipurpose rooms with ancillary toilet facilities, a small kitchenette and reception. The rooms are generously sized ranging from 50 sq.m to 118 sq.m approx. The facility is suitable for a single operator or shared occupancy with individual rooms having a common use or a variety of uses.

7.2 Community Space No. 2

This space is located at ground floor in Block B on the prominent corner on approach along Bannow Road. The space measure 78sqm and has been set out as a single general purpose space suitable for small community groups, classes and meetings. Its has an open plan layout, incorporating a small kitchenette area and WC.



Image 43- View of Entrance to Community Space No.1

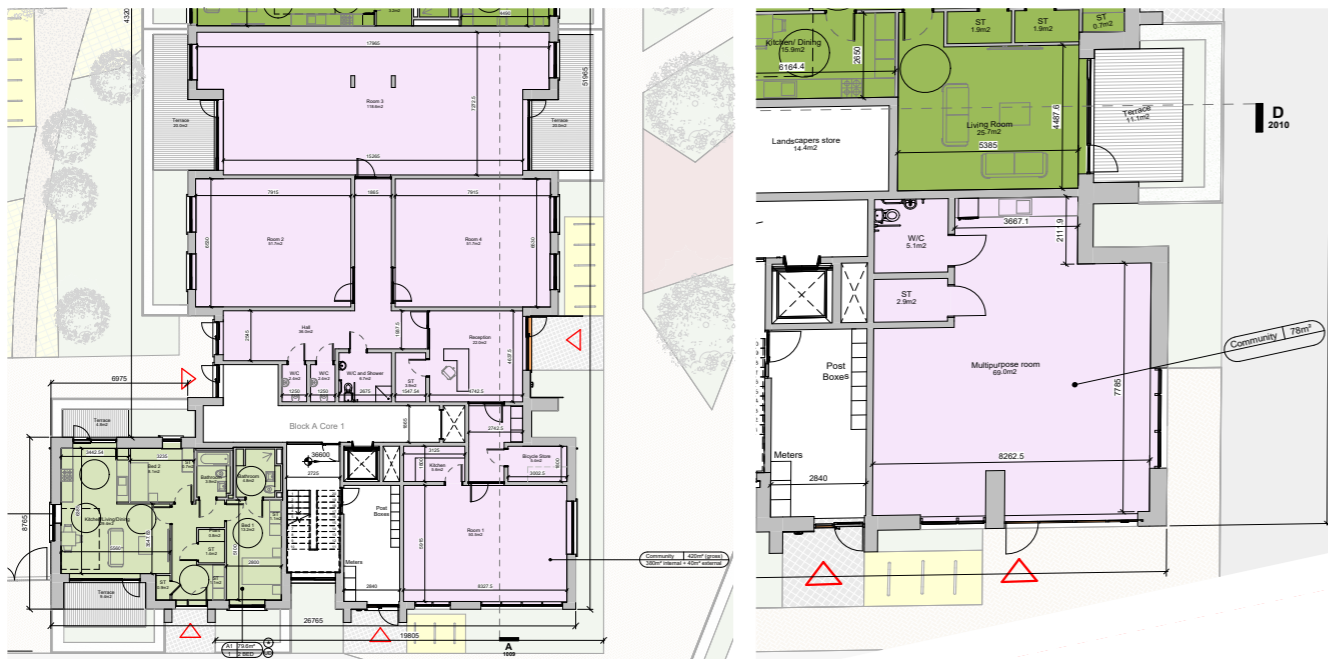


Image 44- Plan of Community Space No. 1 (left) and Community Space No. 2 (right)(shaded pink)

8.0 Materials and Finishes

8.1 Proposed Materials

Cabra housing has a distinct character with painted wet dashed rendered houses of various colours. A feature of this part of Cabra is that corner houses are picked out in a red brick finish.

The existing building on site and the main building on the Batchelors site have a distinct industrial style with red brick facades and elongated horizontal glazing.

For this project we propose a light buff brick for the building as a contrast to some of the other materials in the area and to provide variety. The use of brick as the main material will create a high quality durable finish. Darker window frames and metalwork in a grey/black colour provide accentuation and colour is added around the building at the ground floor with contrast brick panels, porches and front doors.

Recessed balconies offer facade punctuation while also offering additional amenity and usability for occupants.

The core entrances are expressed with contrast metal canopies and vertical bands of glazing that create breaks and recesses in the facades.

Windows and doors, will be either aluminium or alu-clad, with a painted or PPC coating for longevity and to minimise maintenance. Window sills are to be pressed metal and parapet cappings are to be powder coated aluminium or reconstituted stone/concrete.

Following discussion with DCC the elevations to the courtyards will also be finished in brick.

Flat Roof areas will be blue roofs and finished with an intensive green roof (minimum 50% of area) or stone ballast build-up. A light grey concrete tile is proposed for the pitched roofs to Bannow Road.



Image 45 - Springvale Grove, Dublin - Dublin City Architects - gable fronted facade



Image 46 - Ladyswell Square - ABK Architects - proposed light buff brick



Image 47 - Charlemont Regeneration - MCORM Architects - own door units at ground level



Image 48 - Bunratty Walk, Dublin City Architects - recessed balconies



8.2 Facade Detail

The image opposite shows some of the typical materials and finishes proposed as per the palette below:

1. Light buff brick with off white mortar.
2. Grey brick stack bond.
3. Aluminium / aluclad windows and doors
4. Porch/ entrance detail in dark grey brick
5. Front door to ground floor unit in selected colour
6. Powdered coated metal balcony balustrade.
7. Low railing and hedge to mark defensible space
8. Permeable paving
9. Concrete footpath
10. Mixed shrub planting and native trees

Image 49 - View from corner of Forbes Lane and Marrowbone Lane



Image 50 - Boundary Treatment Plan

8.2 Boundary Treatments

The following is a list of the proposed boundary treatments. Please also refer to Dwg No. 8003

Along the west boundary of the site a new 1.8m tall blockwork rendered wall is proposed to replace the existing palisade fence in that location.

To the north a 1.8m railing is proposed to separate the development from the area to be reserved for the future LUAS expansion works. Note the existing boundary onto the railway line along the north side of the reserved area is to be retained and no works are proposed on that boundary. It currently consists of a part weldmesh fence and part blockwork wall.

Along the eastern boundary the existing reinforced concrete wall in that area is to be retained and an additional railing will be fixed on top to ensure a minimum boundary height of 1.8m once site levels have been graded.

Ground floor units onto public areas are provided with vertical railings and hedging to define the defensible zones around the building perimeter. Terrace areas in the more secure courtyard areas are defined with more simple 3 bar railings and hedging.

The area of the shared street to the north of Block B is subdivided with railings and gates in an attempt to prevent vehicular traffic or motorised vehicles using this area as a short cut. The space is prioritised for pedestrian access. Access for emergency vehicles will be maintained.

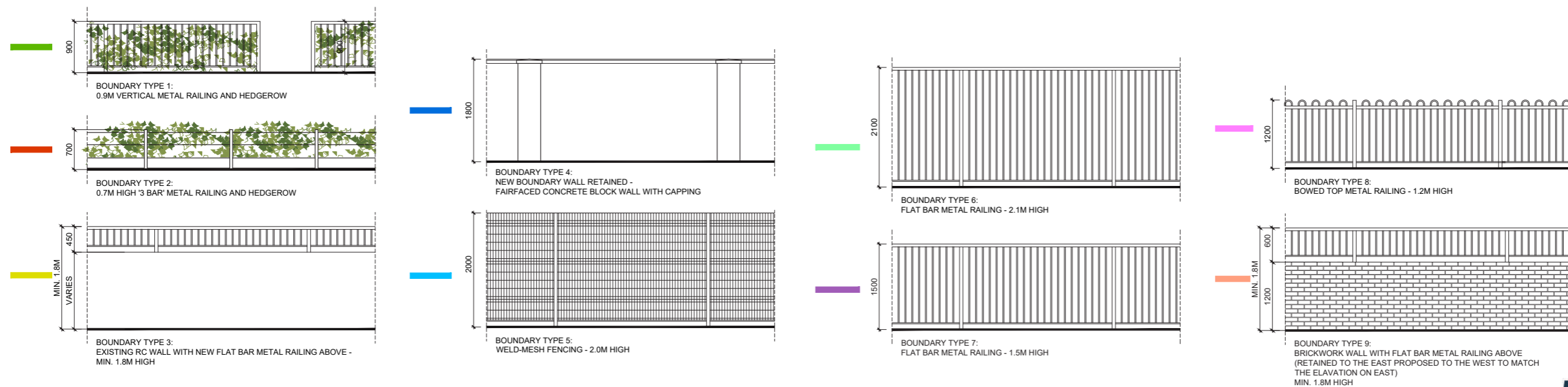


Image 51 - Proposed Boundary Treatments

BANNOW ROAD, CABRA		25/07/25			
24005-MCORM-AR-RP-9001- SCHEDULE OF ACCOMMODATION		Status P3 - PART 8			
SUMMARY					
SITE ANALYTICS :					
1- GROSS SITE AREA:			1.16 hectares		
2- NET SITE AREA:			0.98 hectares		
3- DENSITY:			147units/ha		
4- PLOT RATIO:			1.39		
5- SITE COVERAGE:			40%		
6- RESIDENTIAL MIX					
		Block A	Block B	Overall Totals	
1 BED	No.	18	53	71	
2 BED	No.	21	42	63	
3 BED	No.	1	9	10	
TOTAL	No.	40	104	144	
7- RESIDENTIAL STATS					
		DUAL ASPECT UNITS	No.	76	
		110%+ FLOOR AREA UNITS	No.	121	
		UNIVERSAL DESIGN UNITS	No.	36	
8- RESIDENTIAL AREA TOTALS					
			net	gross	
		RESIDENTIAL FLOOR AREA	sq.m	9817	12523
9- ANCILLARY AREA TOTALS					
			net	gross	
		Community Cultural Space No.1*	sq.m	380	380
		Community Cultural Space No.2	sq.m	78	78
		Internal Bicycle Stores	sq.m	288	288
		Internal Bin Stores	sq.m	87	87
		Plant	sq.m	155	155
		External Bicycle Store	sq.m	22	22
		External Bin Store	sq.m	25	25
		Substation & Switch Room	sq.m	25	25
		TOTAL	sq.m	1060	1060
10- OVERALL AREA TOTALS					
			net	gross	
		TOTAL FLOOR AREA	sq.m	10877	13583
11- PUBLIC OPEN SPACE:					
		Public Open Space	sq.m	900	
		Public Realm Plaza	sq.m	550	
		TOTAL	sq.m	1450	
12- COMMUNAL OPEN SPACE:					
		Block A	sq.m	450	
		Block B	sq.m	850	
		TOTAL	sq.m	1300	
13- BICYCLE STORAGE					
		Long Term Residential spaces	No.	227	
		Short Term Residential spaces	No.	72	
		Long Term Community Cultural spaces	No.	2	
		Short Term Visitor spaces	No.	6	
		TOTAL	No.	307	
14- CAR PARKING					
		Drop Off / Loading		3 No.	
		Go Car		4 No.	
		Resident Parking		25 No.	
		TOTAL		32 No.	

* Community Cultural Space No. 1 is 420 sq.m in total. This consists of 380 sq.m isof internal space and 40 sq.m of associated external terrace space

9.0 Residential Amenity

9.1 Compliance with Internal Design Standards

All apartments have been designed to comply with the Planning Design Standards for Apartments, Guidelines for Planning Authorities, 2025. We note that the scheme was developed up to pre Part 8 stage in accordance with the 2022 Design Standards for New Apartments-Guidelines for Planning Authorities which had more onerous requirements. We suggest that as such we are in compliance / exceed compliance with the recent updates. Please refer to the Housing Quality Assessment included with this application for details of apartment sizes and room areas.

The scheme has also been designed with due regard to the principles of universal design, including the 'Buildings for Everyone' publications. All apartment units are Part M accessible and all upper floors are provided with lift access.

53% of the units are dual aspect and over 84% are sized to be 10% larger than the minimum space standards. All ground floor units have a minimum height of 2.7m. All upper floor units shall have a minimum height of 2.4m.

Storage is provided within each of the units as indicated on the drawings. Minimum storage areas of 3 sq.m for one bed units, 5 sq.m for two bed three person units, 6 sq.m for two bed four person units and 9 sq.m for three bed units are proposed. These areas consist of a mixture of dedicated utility/ store rooms and bathroom storage. In certain areas individual storerooms may be slightly larger than 3. 5sq.m due to the efficiencies of the plan layouts.

Each apartment has a balcony or terrace area accessed from the main living space. The minimum areas provided are 5sq.m for all one bed units, 6 sq.m for all two bed three person units 7 sq.m for all two bed four person units and 9sq.m for the three bed units.

The apartments are generally designed in an efficient plan format balancing the ratio between floor area and external wall which allows for an efficient and sustainable layout while also creating an efficient thermal envelope.

The apartments will be constructed to building regulation standards delivering a high level of energy efficiency in use in line with Part L's near zero energy target. A building lifecycle report has been prepared and is included with the application.

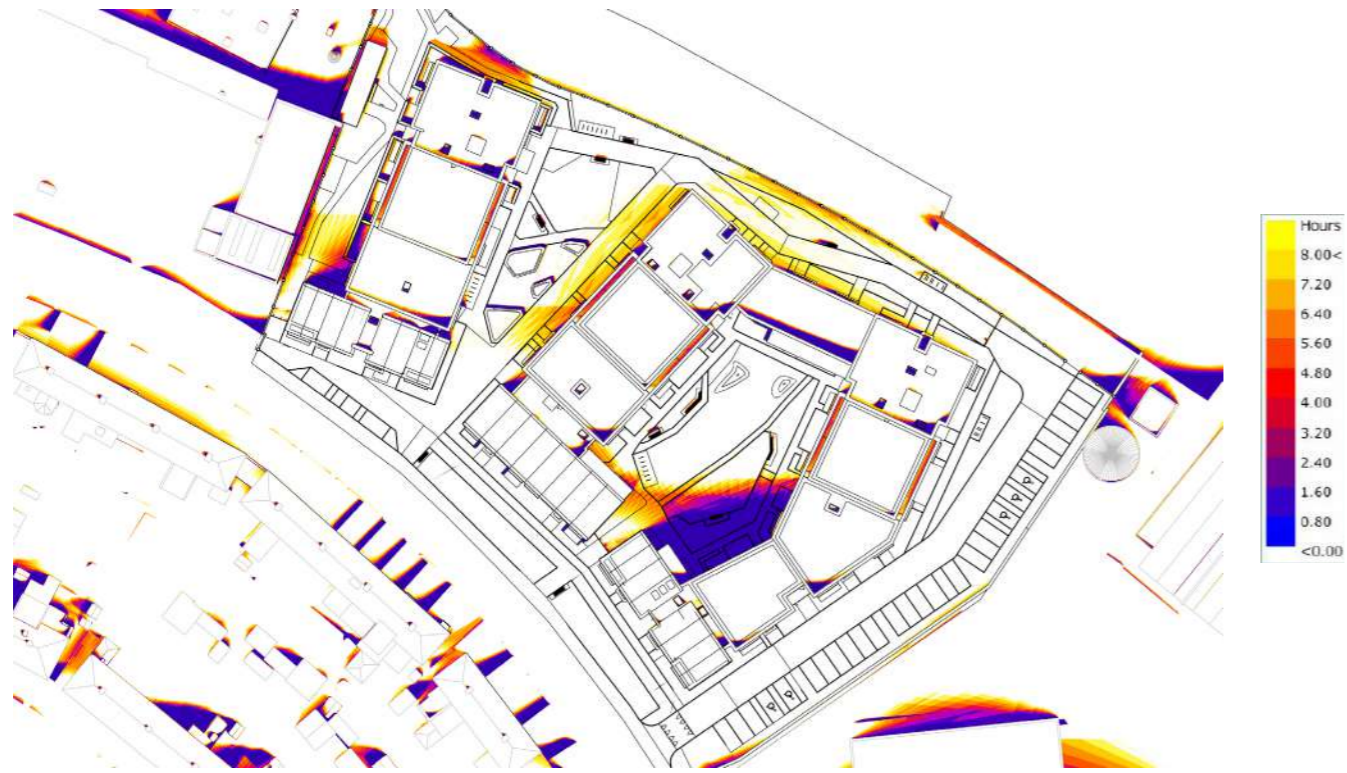


Image 53- Extract from Daylight and Sunlight Report - The Communal courtyard achieves 78.4% sunlight for 2 hours on 21st March



Image 54- Extract from Daylight and Sunlight Report - Daylight Analysis

9.2 Daylight and Sunlight

The provision of adequate daylight and sunlight for the units and the open space areas proposed has been a key part of the design process. The blocks have been designed with due regard to orientation and aspect to ensure high levels of residential amenity for the proposed development. A daylight and sunlight report has been prepared by 3D Design Bureau in accordance with BRE guidance document – ‘Site Layout Planning for Daylight and Sunlight: A Guide to Good Practice’ 2022 Third Edition, (BR209) and British standards BS EN17037 and is included in this application.

Generous separation distances have been provided to properties on surrounding lands. The setbacks from the site boundaries and the proposed layout results in minimal impact on the daylight and sunlight amenity of the surrounding properties. An analysis has been carried out on the closest properties and found that the impact of the development would be minimal on the amenity of these properties.

The proposed public open space and communal open spaces benefit from good access to sunlight well in excess of the minimum requirements. The blocks have been arranged to balance daylight and sunlight while also providing strong urban frontage. The layouts have been planned with due regard to orientation and aspect.

53% of the units are dual aspect. Generous separation distances are provided within the blocks. The courtyard in Block B measures 22m x 28m approx. This results in good access to daylight and sunlight and reduced levels of sky obstruction. The communal courtyard areas in both block exceeds the minimum sunlight guidance levels.

Units generally have either east west or south orientation. There are 3 units in Block B that have a northerly aspect. In accordance with the apartment guidelines north facing units are acceptable when they overlook amenity areas. See image overleaf with views over Greenway, Glasnevin and as far as Howth. These units are also large 1-bed units (55sqm+ / 20% larger than the minimum unit size).

Living rooms and bedrooms are provided with large, glazed openings. 97% (360 No. / 374 No.) of all rooms assessed comply with the minimum daylight recommendations set out in BS EN 17037:2022+A1 with the impact of trees included. Of the rooms (14 no.) that didn't reach the suggested daylight levels 8 No. achieved 80% of the levels or greater and the other 6 No. achieved 50% of the levels or greater.

Any units with a room that falls under the guidance levels for daylight are provided with additional compensatory measures. The units are all at least 10% larger than the minimum sizes and are provided with large areas of glazing. All ground floor units have large private open space areas at least twice the recommended minimum areas. All units meet the minimum requirements for daylight for HPI certification i.e. the window / glazing size for all bedrooms is greater than 10% of the floor area and for living rooms/ kitchens is greater than 15% of the floor area.

Details of the daylight and sunlight analysis are included in the report by 3D Design Bureau and are included with this application.

Howth
O'Connell
Tower
Royal Canal
& Greenway



Image 42a - North facing Unit - Aspect



Image 55 - Dimensions to Surrounding Buildings

9.3 Overlooking, Overbearing, Overshadowing

Generous separation distances have been provided to existing building and within the proposed scheme.

The building line is set back from Bannow Road creating an average distance of 26m to the two storey housing on the opposite side of the street.

Similar separations are provided to the Batchelors factory buildings. Block A is set back 11m from our boundary on the west and we consider this is acceptable as it is half of the traditional 22m back to back separation requirement. The adjoining property at that location is industrial in nature, inward looking and arranged around a central courtyard type space.

The scale and height of the building is all graded towards the railway line from 3 storey to 6 storey.

A Landscape and Visual Impact Study has been prepared by CSR and 3d Design Bureau. The study includes 11 No. verified views of the building from different locations in the surrounding area.

10.0 Access, Car and Cycle Parking

10.1 Compliance with DMURS

The development objectives for the proposed site are to provide for higher-density infill development, in an urban location with good transport links. This helps to fulfill one of the primary objectives of DMURS, which is to encourage more sustainable travel, with pedestrians at the top of the user hierarchy, then cyclists, public transport and finally, private vehicles.

The proposed upgrade works along Bannow Road will provide new wider footpaths, landscaping and planting improving the public realm and prioritising pedestrians.

A pedestrian prioritised site plan is proposed. Vehicular access and parking numbers are restricted and located on the eastern boundary of the site. The provision of a new shared surface street promotes pedestrian and cyclist movement.

A table top ramp and crossing point have been included at the junction at the vehicular site entrance from Bannow Road to slow traffic and to prioritise pedestrians.

10.2 Emergency Access Strategy

The building requires high reach emergency vehicle access to at least 50% of the building perimeter in compliance with DCC Fire Department requirements for buildings over 4 storeys. This is provided along the permeable loop around Block B. The loop shall have readily open-able gates sized to accommodate Emergency vehicles. Details of the EAV route through the shared street is shown opposite. Detailed swept path analysis for other service vehicles - bin collection, delivery van etc. is included in the OCSC planning pack.

10.3 Car Parking

A total of 32no. parking spaces are provided consisting of 3 no. drop off /servicing spaces on Bannow Road and 29 no. residential parking spaces. The 29 No. residential spaces include 4 no. car sharing spaces. A minimum of 16no. EV charging spaces shall be provided. (50% of total spaces)

The site is highly accessible and the design team suggest that this site is an appropriate level of parking for a development of this nature.

Please refer to the Mobility Management Plan prepared by OCSC.



Image 56 - Access for High Reach Emergency Access Vehicles



- Long stay bicycle parking
- Short stay bicycle parking

Image 57 - Bicycle Storage Diagram

10.4 Cycle Parking

A total of 307 No. bicycle parking spaces have been provided - 299 No. residential and 8 No. community/cultural.

The 299 No. residential spaces consist of 227 No. long term spaces (63 in Block A and 160 in Block B) and 72 No. short term external spaces. The 8 no. Community spaces consist of 2 No. long term spaces (1 No. in Community Space A and 1 No. in Community Space 2) and 6 No. short term external spaces.

The Long term residential spaces are provided in 6no. cycle centres as indicated opposite. A mixture of cargo bicycle spaces (13No. / 6%) , Sheffield stands (72 No. / 32%) and double-stacked bicycle racks (142 No. /62%) are proposed as per the tables below. The long term community spaces are provided within each of community spaces. (1 per space)

The layout and arrangement of the proposed Cycle stores is compliant with The National Cycle Manual (2023), in terms of the functional dimensions and quality of parking facilities and the type of access to and from the spaces.

A total of 78 No. Short term cycle parking (residential and community) is provided externally in small clusters at each core entrance and within the communal courtyards. The 78 No. spaces consist of 4 No. cargo bicycle spaces and 74 No. Sheffield stand spaces. 50% of external spaces shall be covered.

Long term Bicycle parking - 229no.
(227 no residential and 2 no. community)

STORE NO.1	No.
Cargo Bikes	2
Sheffield Stand Spaces	8
Double stacker Spaces	18
Total	28

STORE NO.4	No.
Cargo Bikes	0
Sheffield Stand Spaces	0
Double stacker Spaces	16
Total	16

STORE NO.2	No.
Cargo Bikes	4
Sheffield Stand Spaces	20
Double stacker Spaces	36
Total	60

STORE NO.5	No.
Cargo Bikes	3
Sheffield Stand Spaces	16
Double stacker Spaces	20
Total	39

STORE NO.3	No.
Cargo Bikes	4
Sheffield Stand Spaces	20
Double stacker Spaces	36
Total	60

STORE NO.6	No.
Cargo Bikes	0
Sheffield Stand Spaces	8
Double stacker Spaces	16
Total	24

N.B 1 no. bicycle space provided within each Community Space (2 No. Overall)

Short Term Bicycle Parking -78no.
(72 no residential and 6 no. community)

	No.
Cargo Bikes	4
Sheffield Stand Spaces	74
Total	78



- Bin Store
- Bin Collection Point
- Route to Bin Store from core

Image 58 - Bin Storage Diagram

10.5 Operational Waste Management

An Operational Waste Management Plan has been prepared by DNV and is included with the application.

They have identified the following waste requirements for the scheme:

Block	Number of Bins Required for Weekly Collection			
	MMW	Organic	DMR	Glass
Apartments	6 x 1100L	11 x 140L	11 x 1100L	4 x 140L

Block	Number of Bins Required for Weekly Collection			
	MMW	Organic	DMR	Glass
Community Space	1 x 1100L	2 x 140L	1 x 1100L	1 x 140L

Three bin stores have been provided - two for residential use and one for community facility use.

All residents have ease of access to the bin store locations. The maximum travel distance from a core is approx 35 meters.

Bin collection points have been identified adjacent to Bin Store 2 and close to the entrance to the Block A communal open space.

STORE NO.1	No.
Eurobin - 1100 litre	5
Wheelie Bin - 140 litre	4

STORE NO.2	No.
Eurobin - 1100 litre	12
Wheelie Bin - 140 litre	11

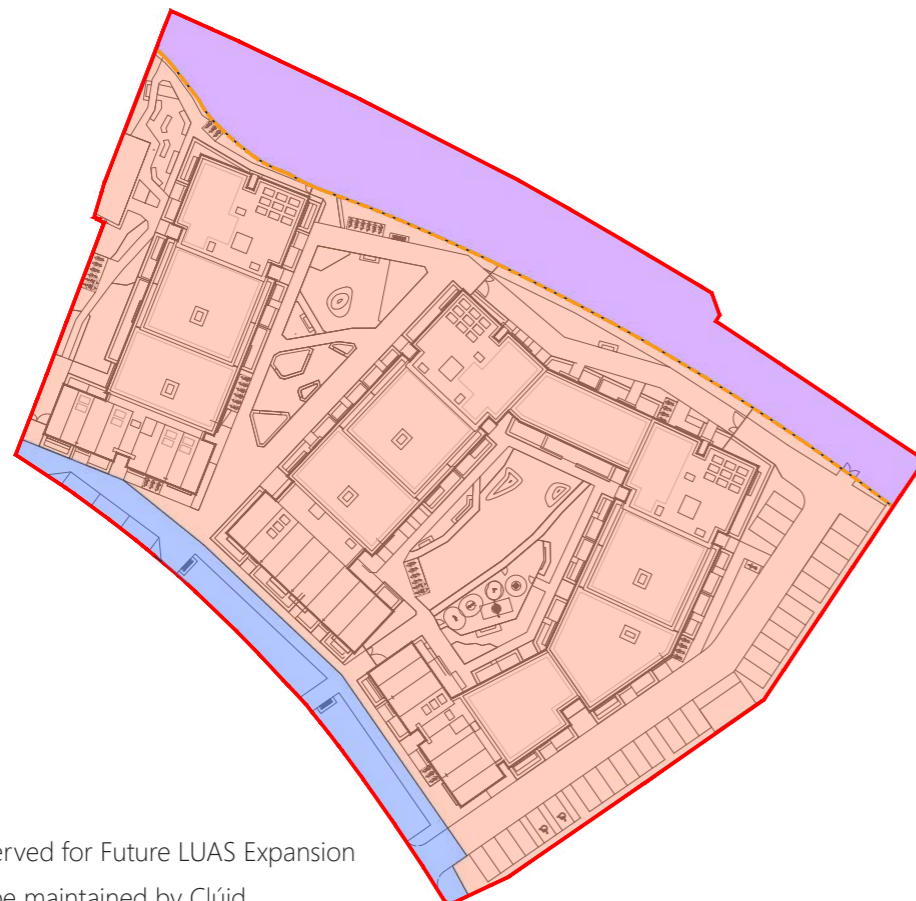
STORE NO.3	No.
Eurobin - 1100 litre	2
Wheelie Bin - 140 litre	3



Image 59 - Plant location - Roof Plan



Image 60 - Plant location - Ground Floor



- Area reserved for Future LUAS Expansion
- Area to be maintained by Clúid
- Area to be Taken in Charge

Image 61 - Taken in Charge Map

10.6 Plant

A centralised plant area is located in Block B in the centre of the overall development. This area will accommodate centralised water tanks and sprinkler tanks for the scheme.

Areas have been indicated at roof level for the possible PV panel arrays. Final details will be subject to detailed design to determine renewable requirements for the development.

It is proposed that units will be provided with individual heating systems and additional space has been provided in each unit to accommodate this plant.

10.7 Taken in Charge

The site will be split into three areas of management.:

- The public footpath and planting areas on Bannow Road will be offered for taking in charge
- The main body of the proposed scheme will be maintained by Clúid Housing.
- The area to the north of the site will remain in DCC ownership and shall be set aside for the proposed LUAS expansion.

10.8 Noise Impact Assessment

A noise impact assessment has been prepared by Traynor Environmental and is included with the application. The noise impact from the railway lines have been assessed.

It concludes that acceptable amenity can be achieved within the development by providing an improved glazing and ventilation specification for units facing onto the tracks. This is a common approach for building near a railway line.

The report also notes that noise levels may become raised on external balconies facing the tracks. However in accordance with recommended guidance for such circumstances all units also have access to alternative outdoor amenity space in the communal courtyard areas which are sheltered from the railway.

It is noted that the new apartment guidelines allow for the omission of balconies in certain circumstances. We have not sought to use this guidance and have provided balconies / private open space for all units.



Image 62 - View of the proposed development from the north

11.0 Urban Design Rationale/ Design Criteria

The design rationale outlined below identifies the key issues considered during the design process in compliance with key indicators of quality urban design and placemaking set out in Section 4.4 of Sustainable Residential Development and Compact Settlements Guidelines for Planning Authorities 2024.

As there is significant overlap here with the Community Safety Strategy, that section of this report should also be consulted. (Section 14.0)

11.1 Sustainable and Efficient Movement

PERMEABLE AND LEGIBLE NETWORK OF STREETS AND SPACES WITHIN THE SITE

Will the plan or development proposal establish a highly permeable and legible network of streets and spaces within the site that optimises movement for sustainable modes of transport (walking, cycling and public transport)?

The existing site is gated and secured by boundary walls and fencing. Access is in and out via two gated entrances on Bannow Road.

The proposed scheme will remove the solid perimeter boundaries creating active street frontage. Street widening and public realm improvement works is also proposed on Bannow Road. A new shared surface street is also provided within the site creating a new permeable route around Block B.

The site is located close to multiple modes of public transport such as Dublin Bus, the LUAS and Irish Rail. The layout allows for ease of movement in and around the site.

CONNECTIONS WITH AND BETWEEN ESTABLISHED COMMUNITIES, SERVICES AND OTHER USES

Have opportunities to improve connections with and between established communities been identified and responded to with particular regard to strategic connections between homes, shops, employment opportunities, public transport, local services and amenities?

The site is located in Cabra West, conveniently situated near several public transport hubs. Due to its strategic location, it benefits from numerous local employment opportunities and access to various educational institutions catering to all age groups. Additionally, the proposed public open space within the new development will provide a valuable resource not only for the residents but also for the surrounding community. The new development will expand the existing residential community on Bannow Road.

STREETS DESIGNED IN ACCORDANCE WITH DMURS

Are streets designed (including the retrofitting of existing streets adjacent to or on-route to the site, where appropriate) in accordance with DMURS to calm traffic and enable the safe and comfortable movement of vulnerable users?

The proposed development will feature vehicular access along the southeast boundary, designed as a shared surface where pedestrians and cyclists will be given priority. The rest of the site will be entirely pedestrianised, with the exception of emergency vehicle access. The frontage along Bannow Road will be set back to create a wider landscaped area between the road and the pedestrian zone, further enhancing the public realm.

QUANTUM OF PARKING MINIMISED

Has the quantum of parking been minimised (in accordance with SPPR4 where relevant) and designed and located in a way that seeks to reduce the demand for private car use, promote sustainable

modes of transport and ensure that the public realm is not dominated by parked vehicles?

A total of 29 no. car parking spaces (including 4 No go car sharing spaces) plus 3no. drop off and servicing spaces have been provided. Vehicular access is limited within the development and a highly pedestrian orientated public realm is proposed.

11.2 Mix of Land Uses (Vibrant Centres and Communities)

MIX AND INTENSITY OF LAND USES APPROPRIATE TO THE SITE

Is the mix and intensity of land uses appropriate to the site and its location and have land uses been distributed in a complementary manner that optimises access to public transport, amenities and local services via walking or cycling?

The site is zoned Z1 'Sustainable Residential Neighbourhoods' (to protect, provide, and improve residential amenities). A housing scheme with complimentary community facility spaces is proposed.

DIVERSE AND VARIED RANGE OF HOUSING TYPES:

Have a diverse and varied range of housing types been provided to meet local and projected needs (having regard to the Housing Need Demand Assessment), supplemented by an innovative range of housing typologies that support greater housing affordability and choice?

The proposed development is for general needs housing. The brief has been derived from a Housing Needs Demand Assessment of the area. The resultant mix of 1, 2 and 3 bed apartments provide for those needs.

Furthermore, in compliance with DCCDP, in excess of 25% of the units have been designed in accordance with Universal Design Principles.

SUPPORT THE REGENERATION AND REVITALISATION OF AN EXISTING CENTRE OR NEIGHBOURHOOD

Will the plan or development proposal supplement and/or support the regeneration and revitalisation of an existing centre or neighbourhood, including the adaption and re-use of the existing building stock in order to reduce vacancy and dereliction (where applicable) and promote town centre living (where applicable)?

The site is located in an area earmarked for regeneration under the Ballyboggan LAP. This proposal will deliver a compact sustainable development in accordance with the objectives for the area.

ENHANCEMENT OF THE PUBLIC REALM

Is the regeneration and revitalisation of an existing centre or neighbourhood supported by the enhancement of the public realm so as to create a more liveable environment, attract investment and encourage a greater number of visitors (where applicable)?

The facades along Bannow Road will be set back to create a new landscaped zone between the road and the development. This area will seamlessly connect to a new public open space located between Blocks A and B, featuring planters and seating areas for the wider community to enjoy. These additions will significantly enhance the public realm, contributing to the overall quality and accessibility of the area.

11.3 Green and Blue Infrastructure (Open Space, Landscape and Heritage)

POSITIVELY RESPONDED TO NATURAL FEATURES & LANDSCAPE CHARACTER

Has the plan or development proposal positively responded to

natural features and landscape character, with particular regard to biodiversity, vistas and landmarks and the setting of protected structures, conservation areas and historic landscapes?

The site is a brownfield site, with a vacant warehouse/ factory and covered largely with concrete and tarmac hardstanding. There are little to no soft landscaping or natural features within the site.

Proposals for new public and communal open spaces, tree planting, intensive green roofs and soft landscaping will therefore serve to improve landscaping and biodiversity.

The subject development proposes a net biodiversity gain.

A COMPLEMENTARY AND INTERCONNECTED RANGE OF OPEN SPACES, CORRIDORS AND PLANTED/ LANDSCAPED AREAS

Have a complementary and interconnected range of open spaces, corridors and planted/ landscaped areas been provided, that create and conserve ecological links and promotes active travel and healthier lifestyles?

The subject site is a vacant industrial site with surrounding development on all sides. There are very little planted / landscaped areas or corridors in the immediate vicinity. The railway tracks to the north create a significant boundary to the Royal Canal Greenway beyond. The proposed scheme looks to redress the balance along Bannow Road providing pockets of green space and tree planting to public areas.

PUBLIC OPEN SPACES UNIVERSALLY ACCESSIBLE AND DESIGNED TO CATER FOR A RANGE OF ACTIVE AND PASSIVE RECREATIONAL USES

Are public open spaces universally accessible and designed to cater for a range of active and passive recreational uses (taking account of the function of other spaces within the network)?

The site levels are to be graded to create a relatively flat site that ties in with the footpaths on Bannow Road. This will facilitate ease of access throughout the site.

INTEGRATED NATURE-BASED SOLUTIONS FOR THE MANAGEMENT OF URBAN DRAINAGE

Does the plan or development proposal include integrated nature-based solutions for the management of urban drainage to promote biodiversity, urban greening, improved water quality and flood mitigation?

The design features extensive active SUDs measures, including tree pits, rain gardens, green and blue roofs, and permeable paving. Interception storage is proposed through a combination of the blue roofs and the permeable paving solutions. A detailed engineering report has been prepared by OCSC and is included with the application.

11.4 Responsive Built Form

COHERENT AND LEGIBLE URBAN STRUCTURE IN TERMS OF BLOCK LAYOUTS AND BUILDING HEIGHTS

Does the layout, orientation and scale of development support the formation of a coherent and legible urban structure in terms of block layouts and building heights with particular regard to the location of gateways and landmarks, the hierarchy of streets and spaces and access to daylight and sunlight?

The design has been developed to integrate into the surrounding context. 3 storey elements face onto Bannow Road stepping up to 6 storeys facing the railway tracks and the canal. The buildings and open spaces have been orientated to maximise daylight and sunlight and for ease of way finding around the site.

BUILDINGS ADDRESS STREETS AND SPACES

Do buildings address streets and spaces in a manner that will ensure they clearly define public and private spaces, generate activity, maximise passive surveillance and provide an attractive and animated interface?

Own door units are provided at ground level to create increased activity and vibrancy to the streets. A defensible zone of soft planting is maintained around the building creating a clear buffer between public and private areas.

Balconies and windows are provided to all facades facing onto public and communal areas creating full passive surveillance of the development and the existing public streets. The central public green space is overlooked on both sides to ensure strong passive surveillance in this area.

LAYOUT, SCALE AND DESIGN FEATURES OF NEW DEVELOPMENT RESPOND TO PREVAILING DEVELOPMENT PATTERNS (WHERE RELEVANT)

Does the layout, scale and design features of new development respond to prevailing development patterns (where relevant), integrate well within its context and provide appropriate transitions with adjacent buildings and established communities so as to safeguard their amenities to a reasonable extent?

The site is located on the edge of residential development in fringe area traditionally occupied by large industrial buildings. The proposal integrates with the existing housing and the surrounding site context and the opportunities offered by this unique location.

COHERENT ARCHITECTURAL AND URBAN DESIGN STRATEGY

Has a coherent architectural and urban design strategy been

presented that will ensure the development is sustainable, distinctive, complements the urban structure and promotes a strong sense of identity?

A light buff/grey brick building is proposed with crisp facades and recessed balconies. The building height and form is proportioned to respond to the existing context while also aiming to maximise the potential of the site.

A coherent architectural language of fenestration, materiality and landscaping will provide a high quality modern development for this location.

12.0 Height/ Density/Scale - Performance Criteria

The design rationale outlined below identifies the criteria to be considered during the design process as set out in the Urban Development and Building Heights Guidelines for Planning Authorities Section 3.

12.1 AT THE SCALE OF THE RELEVANT CITY/TOWN

WELL SERVED BY PUBLIC TRANSPORT INFRASTRUCTURE

The site is well served by public transport with high capacity, frequent service and good links to other modes of public transport.

The site is an accessible location close to the LUAS and train (10 min) and bus services on Carnlough Road and Faussagh Avenue.

INTEGRATING INTO THE EXISTING CONTEXT AND ENHANCING IT AND THE PUBLIC REALM

• Development proposals incorporating increased building height, including proposals within architecturally sensitive areas, should successfully integrate into/ enhance the character and public realm of the area, having regard to topography, its cultural context, setting of key landmarks, protection of key views.

Such development proposals shall undertake a landscape and visual assessment, by a suitably qualified practitioner such as a chartered landscape architect

A LVIA has been prepared by CSR and is included with the Application

CONTRIBUTION TO PLACE-MAKING WITH SENSITIVITY TO EXISTING CONTEXT

On larger urban redevelopment sites, proposed developments should make a positive contribution to place-making, incorporating new streets and public spaces, using massing and

height to achieve the required densities but with sufficient variety in scale and form to respond to the scale of adjoining developments and create visual interest in the streetscape.

A new area of improved public realm is created on Bannow Road by setting back the facade and creating space for planting. The gable fronted facades will create a distinct style and character for the development. The building heights have been graded from 3 storeys to 6 storeys to respond to the context.

12.2 AT THE SCALE OF DISTRICT/ NEIGHBOURHOOD / STREET

ENHANCES URBAN STREETScape AND INTEGRATES WITH SURROUNDINGS

The proposal responds to its overall natural and built environment and makes a positive contribution to the urban neighbourhood and streetscape

The existing site is an abandoned industrial site. This proposal will unlock the potential of the site and provide much needed housing, a creche, a community space and public open space.

ENSURE DIVERSITY OF FORM WITH CONSIDERED MATERIALS

The proposal is not monolithic and avoids long, uninterrupted walls of building in the form of slab blocks with materials / building fabric well considered.

Specific care has been taken in relation to the proportion and massing of the elevations to create variety and to modulate the facades to a domestic scale.

ENHANCE PUBLIC SPACES AND KEY ROUTES WITH APPROPRIATE SCALE

• The proposal enhances the urban design context for public spaces and key thoroughfares and inland waterway/ marine frontage, thereby enabling additional height in development form to be favourably considered in terms of enhancing a sense of scale and enclosure while being in line with the requirements of "The Planning System and Flood Risk Management – Guidelines for Planning Authorities" (2009).

Opportunities for connection to adjoining lands is limited however a fully permeable loop has been created within the scheme therefore ease of access and wayfinding with the Canal Belt identified for 6 storey height.

IMPROVES LEGIBILITY OF LOCAL AREA AND SITE

The proposal makes a positive contribution to the improvement of legibility through the site or wider urban area within which the development is situated and integrates in a cohesive manner.

The improved public realm on Bannow Road will provide greening to a street dominated by hardstanding. The development will become a landmark along the street and draw activity particularly once the creche is operational.

The new public open space will also create a focal point in the neighbourhood.

CONTRIBUTES TO MIX OF USES IN THE AREA

The proposal positively contributes to the mix of uses and/ or building/ dwelling typologies available in the neighbourhood.

The area is dominated by traditional 2 storey housing. This scheme will provide a new variety of unit typologies in the area including 25% universal accessible units. The new community spaces will also have positive contribution to the local community.

12.3 AT THE SCALE OF THE SITE/ BUILDING

FORM OPTIMIZES FOR LIGHT, VENTILATION AND MINIMAL OVERSHADOWING

The form, massing and height of proposed developments should be carefully modulated so as to maximise access to natural daylight, ventilation and views and minimise overshadowing and loss of light.

The development maximises the site characteristics providing local scale buildings closer to the existing properties and higher scale development where large separation distances exist. The building and open space are also designed to maximise access to daylight and sunlight.

QUANTITATIVE PERFORMANCE APPROACH TO DAYLIGHT PROVISION

Appropriate and reasonable regard should be taken of quantitative performance approaches to daylight provision outlined in guides like the Building Research Establishment's 'Site Layout Planning for Daylight and Sunlight' (2nd edition) or BS 8206-2: 2008 – 'Lighting for Buildings – Part 2: Code of Practice for Daylighting'.

A daylight and sunlight assessment has been prepared by 3D Design Bureau and is included with this application. The report demonstrates that the development will have negligible impact on existing properties in the area and the high levels of daylight and sunlight amenity will be achieved within the scheme.

ADDRESS DAYLIGHT SHORTFALLS WITH COMPENSATORY DESIGN AND PLANNING CONSIDERATIONS

Where a proposal may not be able to fully meet all the requirements of the daylight provisions above, this must be clearly identified

and a rationale for any alternative, compensatory design solutions must be set out, in respect of which the planning authority or An Bord Pleanála should apply their discretion, having regard to local factors including specific site constraints and the balancing of that assessment against the desirability of achieving wider planning objectives. Such objectives might include securing comprehensive urban regeneration and or an effective urban design and streetscape solution.

Three Criteria were assessed:

- impact on surroundings
- sunlight in open spaces
- daylight in habitable rooms

Items (i) and (ii) met the requirements of BS816-2:200 and surpassed them

Of the 1100 rooms assessed for daylight 95% met the suggested daylight levels. 5% fell slightly below.

12.4 SPECIFIC ASSESSMENTS

To support proposals at some or all of these scales, specific assessments may be required and these may include:

Specific impact assessment of the micro-climatic effects such as downdraft. Such assessments shall include measures to avoid/mitigate such micro-climatic effects and, where appropriate, shall include an assessment of the cumulative micro-climatic effects where taller buildings are clustered.

A micro-climate report has been prepared and is included with the application

In development locations in proximity to sensitive bird and / or bat areas, proposed developments need to consider the potential interaction of the building location, building materials and artificial lighting to impact flight lines and / or collision.

Due to proximity to the LUAS and train lines the site is not a bird sensitive area. Please refer to the ecological impact assessment included with this application. A bat survey is included with the application.

An assessment that the proposal allows for the retention of important telecommunication channels, such as microwave links.

The building is of modest scale, 6 storeys, which is in accordance with the Buildings heights guidelines. We don't consider there will be any impact on the above.

An assessment that the proposal maintains safe air navigation.

The site is not in an airport zone and is of modest height.

An urban design statement including, as appropriate, impact on the historic built environment

This report is the Architects Design Statement and it includes the urban design rationale.

Relevant environmental assessment requirements, including SEA, EIA, AA and Ecological Impact Assessment, as appropriate.

Refer to reports prepared by DNV and included in this application.

12.5 DCC Development Plan Appendix 3 - Table 3

Performance Criteria in Assessing Proposals for Enhanced Height, Density and Scale- Checklist

1. To promote development with a sense of place and character

The proposed scheme has been developed to create a new sustainable development. The scale and massing of the buildings has been carefully apportioned to respect the character of the area and to deliver on the potential of the site characteristics. A distinct gable fronted three storey facade is proposed to Bannow Road which is set back from the street providing space for street greening and public space creating a sense of place and character.

2. To provide appropriate legibility

A new approach to the street frontage is proposed. The existing utilitarian concrete streetscape is to be enlivened with planting and landscaping. A strong facade is proposed to Bannow Road with high levels of street activation and overlooking. A new permeable loop is proposed within the site and a new central public open space.

3. To provide appropriate continuity and enclosure of streets and spaces

The streetscape to the northside of Bannow Road is quite disparate due to the previous industrial nature of the lands. This scheme aim to establish a new frontage and template for the street. High levels of street activation and overlooking are proposed for the existing street and the new spaces created within the scheme.

4. To provide well connected, high quality and active public and communal spaces

The scale of development proposed is quite modest. Net site area is less than 1 hectare. A new public open space is created between the two blocks. Separate dedicated communal open spaces are provided for both Blocks.

5. To provide high quality, attractive and usable private spaces

All units are provided with private open space areas in line with the Apartment Guidelines

6. To promote mix of use and diversity of activities

The site is zoned Z1 Sustainable Residential neighbourhood. The scheme includes a total of 144 apartments, a creche and a community space in line with the permissible zoning uses.

7. To ensure high quality and environmentally sustainable buildings

An energy and sustainability report has been prepared by OCSC and is included in this application. A truly environmentally sustainable building draws on many factors from site layout and planning, orientation, ecology, landscaping, biodiversity, SUDs, energy use, embodies carbon and ongoing maintenance and renewal.

It is intended to seek HPI accreditation for the project and the scheme has been designed with HPI in mind.

8. To secure sustainable density, intensity at locations of high accessibility

This is one of the primary considerations in the design stage. The site is highly accessible close to Broombridge Station and the site offers the potential for a high density scheme within the canal boundaries and close proximity to the city centre.

9. To protect historic environments from insensitive development

This part of the city developed from the 1920s under the Dublin Corporation led suburbanization of the city. The subject site was zoned industrial in the original masterplan for the character of the area developed accordingly. Realising the strategic location the area has now been rezoned Z1 sustainable residential and the objective for the area is to see a compact sustainable regeneration. We consider that the proposed scheme is in accordance with the future objectives for the area.

10. To ensure appropriate management and maintenance

The proposed scheme will be managed by Clúid Housing which is an approved housing body with cast experience of managing housing schemes.

13.0 Accessibility (UD Apartments)

Dublin City Council Development Plan (2022-2028) advises that a minimum of 50% of apartments are to exceed minimum area standards by 10%, and that in addition, 50% of apartments that are in excess of minimum size requirements are to be designed to be UD compliant.

The UD standards noted in the DCC development plan are the requirements of the 'Universal Design Guidelines for Homes in Ireland' developed by the Centre for Excellence in Universal Design (National Disability Authority).

Based on the above requirement, the minimum provision of UD apartments would be 25% of total units. The layouts indicate 37 No. units (25.7%) with UD layouts.

It is noted that flexibility has also been designed into other unit types by providing generous unit areas. Alternative UD layouts could be provided for other unit types to ensure that end user requirements can be met and to allow the UD allocation to be adjusted or reassigned to suit end user requirements.

It is noted that where any adjustment occurs a minimum provision of 25% of units shall be achieved.

For all points of approach to individual apartments, universal access has been addressed as follows:

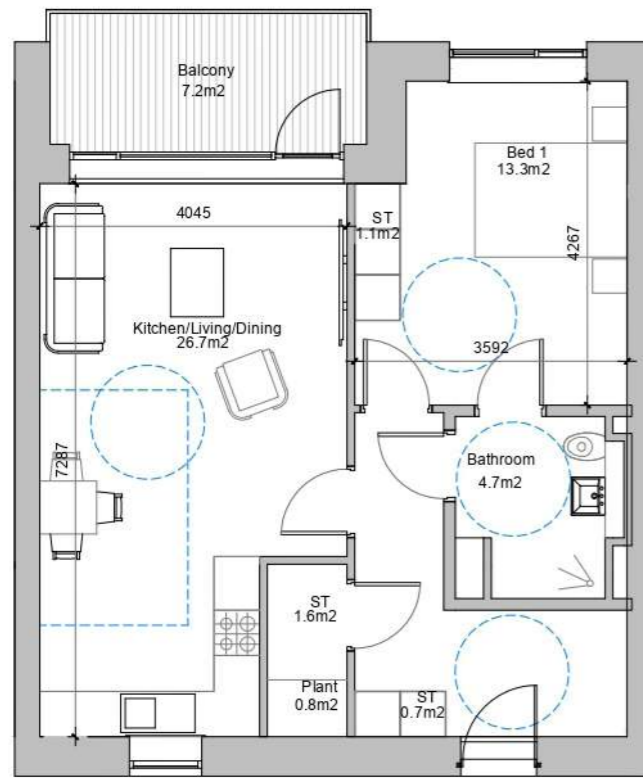
- Approaches to building cores, apartment entrances and ancillary facilities are in compliance with TGD Part M.
- Accessible car parking spaces are located on the shared street for ease of set down. The spaces are close to each core in Block B.
- Stairs and lifts are design to Part M standards.
- All internal corridors have been designed with 1.8m width passing points.

The units indicated with UD layouts consist of:

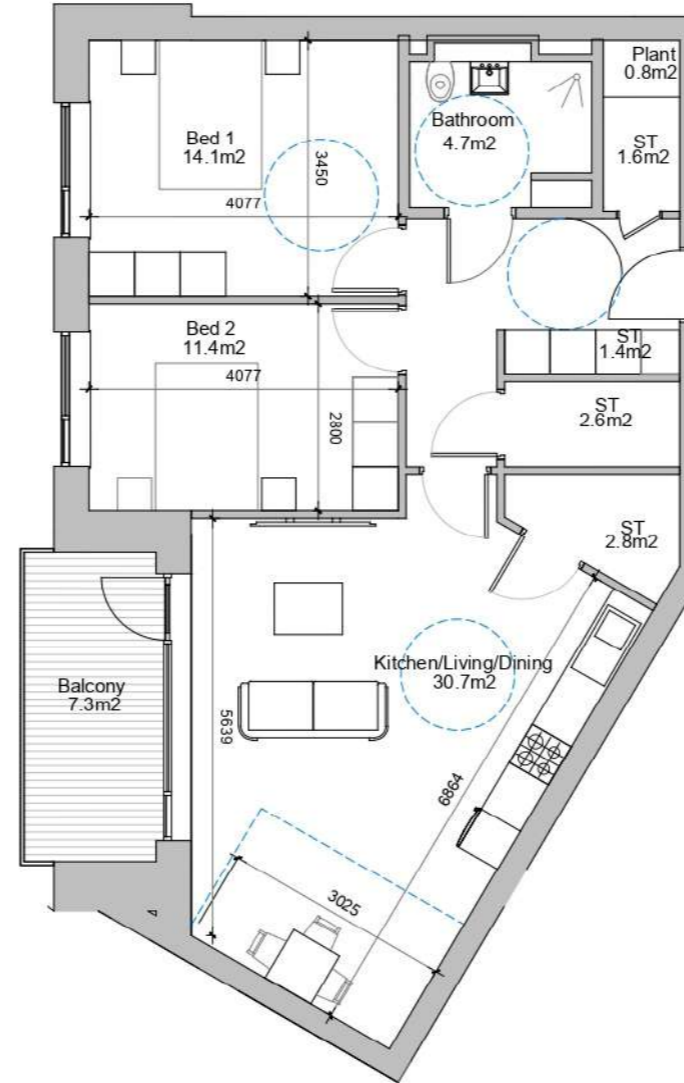
- 1 Beds (11 no.)
- 2 Beds (19 no.)
- 3 Beds (7 no.)

The criteria addressing the design of the internal unit layouts are summarised as shown below:

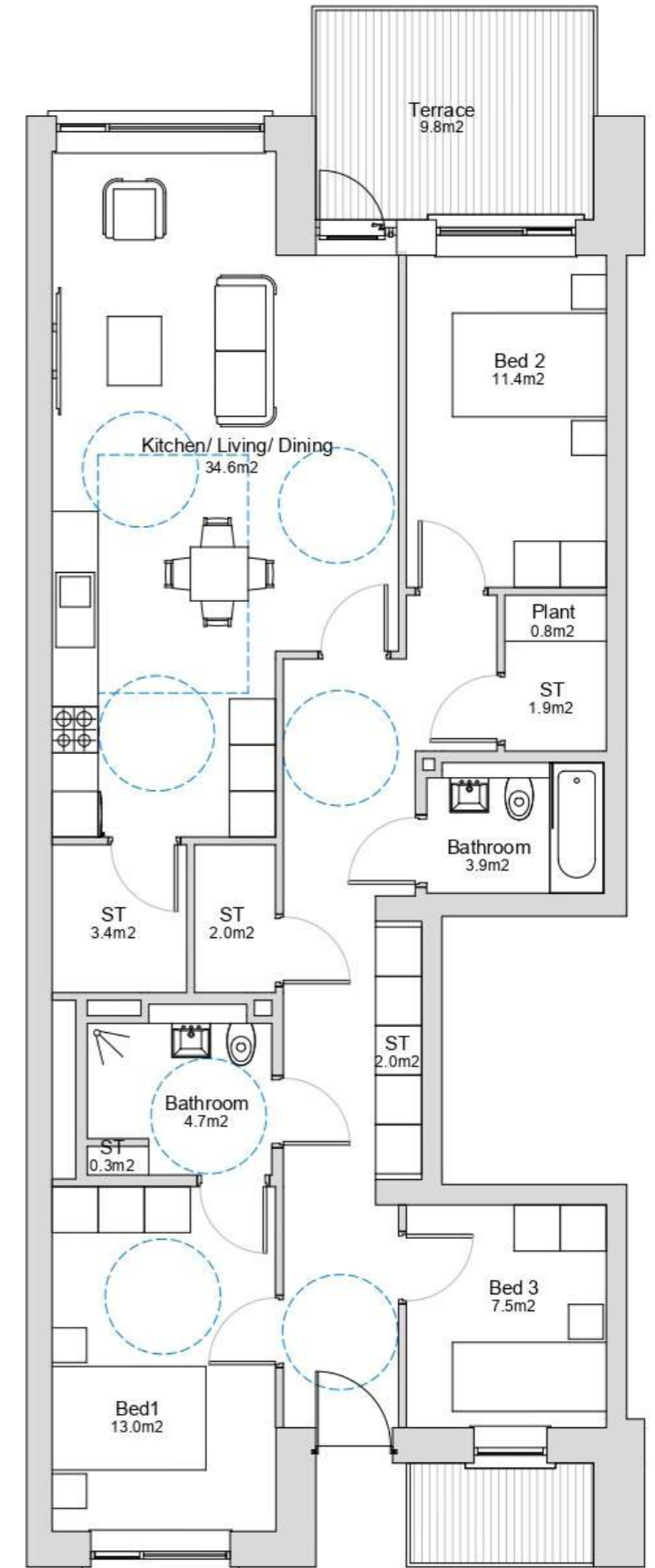
1. Large and flexible Kitchen/ Dining/ Living Room with ample unobstructed space to access all areas for everyone with ease of movement through the kitchen
2. Minimum 800mm wide clear route between furniture and in front of windows and routes between doors
3. 1200mm clear space on three consecutive sides of a table
4. Kitchen is not a thoroughfare. Cooker / hob and sink are in the same run of worktop.
5. The kitchen space located next to the dining area to ease access for carrying food and crockery.
6. Large and accessible bathroom door opens outwards, with level access shower
7. Bathroom adjacent to the main bedroom with flexibility to provide direct access from the bedroom.
8. Clear access space of 800mm on both sides and at the end of the double bed.
9. Provide a clear space for a turning circle of 1500mm
10. Doors open into rooms (such as living rooms, bedrooms and
11. kitchens) with hinge-side of the door is adjacent to a return wall and approx. 300mm clear space on the leading edge of doors
12. Entrance door with a clear width of circa 1000mm; with 1200mm x 1200 mm clear landing.



Typical Universal Design One Bed



Typical Universal Design Two Bed



Typical Universal Design Three Bed

14.0 Community Safety Strategy

This report addresses the requirements of the Dublin City Development Plan, 2022-2028, Chapter 15.4.5 - Safe and Secure Design. This requirement states:

"... All residential developments shall refer to Design for Safety and Security' guidance contained in the DEHLG 'Quality Housing for Sustainable Communities – Best Practice Guidelines for Delivering Homes Sustaining Communities' (2007).

New developments and refurbishments should be designed to promote safety and security and avoid anti-social behaviour by:

- Maximising passive surveillance of streets, open spaces, play areas and surface parking.
- Avoiding the creation of blank facades, dark or secluded areas or enclosed public areas.
- Eliminating leftover pockets of land with no clear purpose.
- Providing adequate lighting.
- Providing a clear distinction between private and communal or public open space, including robust boundary treatment.
- Enabling residents to watch over the entrance to their home; recessed entrances should be avoided and front doors should also be overlooked from other houses or from well-trafficked public areas.
- Locating back gardens next to other back gardens or secure private areas rather than onto roadways or other public areas.
- Ensuring that the layout and design of roads within residential areas encourages appropriate traffic volumes and speeds.
- Providing clear and direct routes through the area for pedestrians

and cyclists with safe edge treatment, maintaining clear sight lines at eye level and clear visibility of the route ahead.

- Using materials in public areas which are sufficiently robust to discourage vandalism.
- Avoiding the planting of fast-growing shrubs and trees where they would obscure lighting or pedestrian routes; shrubs should be set back from the edge of paths.
- Consulting with An Garda Síochána crime prevention design advisor where appropriate; Dublin City Council will also have regard to the Guidelines on Joint Policing Committees as established under the Garda Síochána Act 2005 as amended (2014), in order to ensure safe and secure communities.

On housing developments over 100 units, the Council will require the submission of a Community Safety Strategy (see policy QHSNO12) which would set out the design features incorporated to address the above measures to ensure a high level of safety and security is maintained including, overlooking, passive surveillance, street lighting and clear accessible routes."

For the following sections, we have used the bullet points above as key headings.

13.1 MAXIMISING PASSIVE SURVEILLANCE OF STREETS, OPEN SPACES, PLAY AREAS AND SURFACE PARKING

All publicly accessible spaces, including perimeter streets, and the public open space to the building frontage are overlooked by multiple apartment windows, patio doors and balconies, creating strong opportunities for passive surveillance. The principal spaces are addressed separately as follows:

PUBLIC OPEN SPACE:

The public open space is situated between Blocks A and B, with views

from both sides, ensuring it is fully overlooked. The communal open spaces at the center of Block B and to the west of Block A will be gated, providing access for residents only. A children's play area is also provided within the courtyard of Block B

CAR PARKING:

There will be 3 no. drop off spaces provided in front of Block A. There will be 29 no. residential spaces provided to the East of Block B.

Permeability:

A full permeable loop is provided around Block B avoiding cul de sacs and dead end arrangements. Access is provided to both blocks and own door units activate the new streetscape.

13.2 AVOIDING THE CREATION OF BLANK FACADES, DARK OR SECLUDED AREAS OR ENCLOSED PUBLIC AREAS.

All facades are activated for passive surveillance, there are no long extents of blank facade. 53% of the units are dual aspect ensuring all public open spaces are overlooked.

13.3 ELIMINATING LEFTOVER POCKETS OF LAND WITH NO CLEAR PURPOSE.

The site has a fairly regular shape and the layout provides a clear delineation of space from public to communal to private. No residual space are created.

13.4 PROVIDING ADEQUATE LIGHTING.

A public lighting scheme has been prepared for all public areas and the communal courtyard.

13.5 PROVIDING A CLEAR DISTINCTION BETWEEN PRIVATE AND COMMUNAL OR PUBLIC OPEN SPACE, INCLUDING ROBUST BOUNDARY TREATMENT.

A defensible planting zone with own door front garden areas has been provided around the perimeter of the building to create a clear buffer between public and private. The communal open spaces are both enclosed with gated access direct from the public open space as well as access from each of the stair cores and ground floor units.

13.6 ENABLING RESIDENTS TO WATCH OVER THE ENTRANCE TO THEIR HOME; RECESSED ENTRANCES SHOULD BE AVOIDED AND FRONT DOORS SHOULD ALSO BE OVERLOOKED FROM OTHER HOUSES OR FROM WELL TRAFFICKED PUBLIC AREAS.

Own door units are provided at ground floor level to provide street activity and vibrancy. Where own doors are provided they are in prominent locations and are overlooked from units above and/ or opposite.

13.7 LOCATING BACK GARDENS NEXT TO OTHER BACK GARDENS OR SECURE PRIVATE AREAS RATHER THAN ONTO ROADWAYS OR OTHER PUBLIC AREAS.

This is an apartment scheme - no traditional back gardens with 2m high walls are proposed. All private open spaces have low boundaries and visual connection to adjoining spaces. No blank walled areas are created.

13.8 ENSURING THAT THE LAYOUT AND DESIGN OF ROADS WITHIN RESIDENTIAL AREAS ENCOURAGES APPROPRIATE TRAFFIC VOLUMES AND SPEEDS.

Minimal vehicular access is proposed within the development. A shared street is provided that prioritises the pedestrian and the cyclist. Additional traffic calming measures have been provided at the entrance from Bannow road to slow any vehicular traffic and provide a crossing point.

13.9 PROVIDING CLEAR AND DIRECT ROUTES THROUGH THE AREA FOR PEDESTRIANS AND CYCLISTS WITH SAFE EDGE TREATMENT, MAINTAINING CLEAR SIGHT LINES AT EYE LEVEL AND CLEAR VISIBILITY OF THE ROUTE AHEAD.

A quality audit has been prepared by Bruton Consulting Engineers. The audit assesses the provisions for motorists, pedestrian and cyclists. A simple permeable loop is created within the scheme allowing access to both blocks. Vehicular access is restricted to the shared surface street along the eastern boundary. A pedestrian prioritised scheme is proposed.

13.10 USING MATERIALS IN PUBLIC AREAS WHICH ARE SUFFICIENTLY ROBUST TO DISCOURAGE VANDALISM

The surfaces of all public areas have been design to achieve a high quality robust finish. The materials consist of a mixture of permeable paving. Robust street furniture is proposed in the form of raised concrete planters that double up as seating areas. Brick and concrete finishes are proposed to all facades in accessible locations.

13.11 AVOIDING THE PLANTING OF FAST-GROWING SHRUBS AND TREES WHERE THEY WOULD OBSCURE LIGHTING OR PEDESTRIAN ROUTES; SHRUBS SHOULD BE SET BACK FROM THE EDGE OF PATHS.

A public lighting design has been delivered that ensures sufficient lighting of the streets, public spaces and communal spaces. Soft landscaping has been selected with a preference for native species, that are robust, but require only regular maintenance. The planting strategy will include a combination of low level planting, and trees with a crown-lifted canopy that is well above eye level where appropriate. Both the public lighting and landscape designs have been fully coordinated, and are included elsewhere as part of these proposals

13.12 CONSULTING WITH AN GARDA SÍOCHÁNA CRIME PREVENTION DESIGN ADVISOR WHERE APPROPRIATE; DUBLIN CITY COUNCIL WILL ALSO HAVE REGARD TO THE GUIDELINES ON JOINT POLICING COMMITTEES AS ESTABLISHED UNDER THE GARDA SÍOCHÁNA ACT 2005 AS AMENDED (2014), IN ORDER TO ENSURE SAFE AND SECURE COMMUNITIES.

To date the development team have assessed the need for such consultation and have concluded that it is not appropriate given the nature, scale and location of the proposed development. Should such consultation be requested it will be facilitated. However, the design has had regard to the principles of Crime Prevention through Environmental Design (CPTED), in particular through consideration of territoriality and the creation of defensible space

15.0 SUMMARY



Image 63 - View of the typical ground floor treatment

In developing the design proposal for this site at Bannow Road, Cabra, MCORM have endeavoured to apply best practice urban design to the distinct characteristics of the subject site. We have had due regard to the Dublin City Council Development Plan, relevant national policy and guidance together with the feedback provided from the local authority during the preplanning phase.

The site is strategically located along the Canal Belt and the scheme has been developed in accordance with the general parameters set out for building height of 4-6 storeys and density ranges of 100-250 units/ ha.

The site is located at the edge of the 1920's Cabra housing scheme with its unique character of two storey housing ,suburban style streets and in this area the juxtaposition of larger industrial factory buildings.

In summary a total of 144 new residential units and two community spaces are proposed in two blocks ranging from 3-6 storeys.. We submit that the proposed scheme will constitute sustainable residential development which delivers a high-quality design solution and a high level of residential amenity on the subject lands.

The massing of the blocks and set backs from the street have been carefully considered to provide space for the street widening, improvement and greening and to balance the requirements for sunlight and daylight to both the existing properties and the proposed units.

A new permeable loop street is proposed within the scheme incorporating a new public open space between the two blocks .

The proposed materials and elevation composition will result in a strong architectural rhythm. High quality materials are proposed for the building and landscaping throughout ensuring that the development ages well with strong consideration of lifecycle performance.



Image 64 - View of new Public Open Space and permeable loop